

AGENDA CITY OF CEDAR FALLS, IOWA PLANNING AND ZONING COMMISSION MEETING WEDNESDAY, FEBRUARY 13, 2019 5:30 PM AT CITY HALL

Call to Order and Roll Call

Approval of Minutes

1. Planning and Zoning Commission Minutes of January 23, 2019.

Public Comments

Old Business

2. College Hill Neighborhood Overlay District - Site Plan Review: 2119 College Street (DEFERRED)

Location: 2119 College Street, 925 W. 22nd Street, & 1003 W. 22nd Street **Applicant:** CV Commercial, LLC., Owner; Slingshot Architects, Architect

Previous discussion: November 28, 2018, December 12, 2018, January 9, 2019

Recommendation: Recommend Approval

P&Z Action: Item deferred until City Council considers proposed zoning code amendments for the

College Hill Overlay Zoning District

3. Land Use Map Amendment – SE Corner of Union Road and West 12th Street

Location: SE Corner of Union Road and West 12th Street

Applicant: Brent Dahlstrom, Owner; NewAldaya Lifescapes, Fehr Graham Engineering

Previous discussion: 1/23/19

Recommendation: Review and set a date of public hearing.

P&Z Action: Review and set a date of public hearing for the February 27 P&Z meeting

4. Rezoning from A-1 to RP – SE Corner of Union Road and West 12th Street

Location: SE Corner of Union Road and West 12th Street

Applicant: Brent Dahlstrom, Owner; NewAldaya Lifescapes, Fehr Graham Engineering

Previous discussion: 1/23/19

Recommendation: Review and set a date of public hearing.

P&Z Action: Review and set a date of public hearing for the February 27 P&Z meeting

5. Greenhill Village Townhomes II Preliminary Plat

Location: 5.3 acres southeast of the Greenhill Road and Hudson Road intersection

Applicant: Panther Farms LLC - owner; CGA Engineers - Civil Engineer

Previous discussion: 1/23/19

Recommendation: Recommend Approval.

P&Z Action: Review and make a recommendation to City Council

6. Greenhill Village Townhomes II Site Plan

Location: Lot 1 in the proposed Greenhill Village Townhomes II Subdivision **Applicant:** Panther Farms LLC – owner; CGA Engineers – Civil Engineer

Previous discussion: 1/23/19

Recommendation: Recommend Approval.

P&Z Action: Review and make a recommendation to City Council (Note: This item will not be placed on the City Council agenda until a Final Plat is approved).

New Business

7. Nomination and Election of Officers

Commission Updates

Adjournment

Reminders:

Cedar Falls Planning and Zoning Commission Regular Meeting January 23, 2019 City Hall Council Chambers 220 Clay Street, Cedar Falls, Iowa

MINUTES

The Cedar Falls Planning and Zoning Commission met in regular session on Wednesday, January 23, 2019 at 5:30 p.m. in the City Hall Council Chambers, 220 Clay Street, Cedar Falls, Iowa. The following Commission members were present: Adkins, Hartley, Larson, Leeper, Oberle, and Wingert. Giarusso, Holst and Saul were absent. Karen Howard, Community Services Manager, David Sturch, Planner III, and Iris Lehmann, Planner II, were also present.

- 1.) Chair Oberle noted the Minutes from the January 9 regular meeting are presented. Ms. Adkins made a motion to approve the Minutes as presented. Mr. Hartley seconded the motion. The motion was approved unanimously with 6 ayes (Adkins, Hartley, Larson, Leeper, Oberle, and Wingert), and 0 nays.
- 2.) The first item of business was the College Hill Neighborhood Overlay District Site Plan Review for 2119 College Street. This item was deferred until the City Council considers proposed amendments to the College Hill Overlay Zoning District.
- 3.) The next item for consideration by the Commission was a Land Use Map Amendment and rezoning at the southeast corner of Union Road and West 12th Street. Chair Oberle introduced the item and Mr. Sturch provided background information. He explained that there is a request to rezone property 42.3 acres at the southeast corner of W. 12th Street and Union Road from A-1, Agriculture to RP, Planned Residential. It is proposed to build a 55 plus residential community for the NewAldaya campus with multi-unit dwellings. The project would include open space amenities, trails and sidewalks. He discussed right-of-way dedication, as well as sidewalk and future roadway improvements. He discussed the criteria required for rezoning and the need for a land use map amendment. He also spoke about the sanitary sewer and water mains throughout and around the site, as well as the fees that will go along with these. Mr. Sturch talked about roadway access requirements and future improvements, as well as subdivision platting and drainage. Staff would like to discuss the item, gather comments from the Commission and public and continue the discussion to the next meeting.

Scott Hagberg, representative of the Wild Horse Ridge Home Owners Association Board of Directors, stated that the neighborhood would have no issues with the NewAldaya residents moving there, but wanted to know why the project was being considered before FEMA floodplain review is done. There is also concern regarding how much hard space there would be that would create runoff as well as water drainage in the area.

Mark Sigwarth, 1028 Cherrywood Drive, had concerns with the stormwater management, as well as natural and man-made runoff. He questioned water from rain events from the golf course, Robinson Dressler, Wild Horse and Union Road ditch and Cherrywood drainage. He stated that groundwater levels have changed over the past 15-20 years. He also noted concerns with traffic, as well as concerns with the size of the buildings. He believes that the proposed plan does not fit with the surrounding neighborhood and that home values will decrease

Mike Goyen, 1712 Union Road, stated that he also has drainage concerns. Currently he already has issues due to farming practices in the area and feels this will only make them

worse.

Craig Schnathorst, 1120 Cherrywood Drive, also stated concerns with the drainage issues. He noted that he has issues with water in his basement and his lawn doesn't dry out. He also feels the building will be too big for the area.

Mike Girsch, 1608 West Union Road, wants to be sure that the existing drainage will function with additional building in that area.

Mr. Wingert noted that he will be abstaining from this item.

Mr. Leeper asked the developer if there is a broader review of the watershed being done than just on this property. Jon Biederman, Civil Engineer with Fehr-Graham Engineers, stated that they have reviewed the floodplain based on the existing ground. The profile was applied to the existing ground. He stated that there is no intent to reduce or remove the existing floodplain in the project. The developer will maintain the floodplain. The water from the west in those two locations will be transferred through and he sees no issue with it. He stated that they could look at ways to increase capacity to help with the water issues.

Mr. Leeper also asked about future roadway improvements and the affects they may have. He noted that the density at the corner is going to change and increase over time.

Ms. Oberle felt that the biggest issue with continued development is stormwater.

Ms. Howard stated that they will look into the concerns that were presented by the neighbors before the next meeting.

4.) The Commission then considered the Greenhill Village Townhomes II Preliminary Plat. Chair Oberle introduced the item and Ms. Lehmann provided background information. She explained that Panther Farms LLC is proposing to subdivide 5.3 acres of land southeast of the Greenhill and Hudson Road intersection. She displayed the approved concept plan tied to this location and explained that the area is considered medium density residential. The proposed Subdivision Plat is consistent with the concept plan. The proposal consists of one developable lot (Lot 1), tracts for future streets and stormwater basins, and two outlots as placeholders for future development. She discussed the proposed detention basin and the flow of the runoff, sidewalks, and a private alley that will service Lot 1. Utilities are available to the site. Ms. Lehmann noted that another Preliminary and Final Plat will be required before the two outlots can be developed. Staff recommends gathering comments from the Commission at this time and continuing discussion at the next Planning and Zoning meeting.

Mr. Wingert recused himself due to a conflict of interest.

James Denny, 4622 Hudson Road, stated that the area was supposed to be buffered with single-family houses and explained his concerns with traffic and safety issues it would cause.

Tim Hanson, 1517 Athens Court, noted his concerns with decreased property values and water runoff issues.

Ms. Lehmann noted that staff has looked into the water runoff issues and there will be two detention basins that will serve the site and one less proposed townhome then shown on the concept plan. The site will be graded so that no additional water will flow and hinder neighbors.

Mr. Leeper asked if traffic concerns had been looked into. Ms. Lehmann said that staff would look into it.

The discussion of this item will be continued at the next meeting.

- 5.) The next item of business was the Greenhill Village Townhomes II site plan. Chair Oberle introduced the item and Ms. Lehmann provided background information. She explained that this site plan will be contingent upon the previously discussed preliminary plat and the submission and approval of a final plat. The lot is at the southeast corner of Lloyd Lane and Norse Drive intersection. Two townhomes are being proposed on the site.. The proposal closely follows the approved concept plan. She discussed the site plan elements, including height, setbacks, parking, access, sidewalks, landscaping, trash, stormwater management, and signage. She displayed renderings of the proposed building layout and façade elements. Staff would like to bring this to the commission for discussion only at this time.
 - Mr. Wingert recused himself due to a conflict of interest.

Mr. Leeper asked if there is any percentage of masonry requirements. Ms. Lehmann noted that there is not. He stated that it seems to be consistent with the rules we have set in place. Mr. Hartley noted he would like to see more about the water retention and runoff at the next meeting. Ms. Oberle noted that she feels the porches make the front more interesting. Mr. Leeper asked about the parking calculations. Ms. Lehmann stated that they are meeting and exceeding the parking requirement. Mr. Leeper then asked about street parking and whether additional onsite parking could be considered in the alley. Ms. Lehmann will discuss the option with staff and the developer.

Discussion of this item will be continued at the next meeting.

- 6.) Ms. Howard noted that the nomination and election of officers will be on the agenda at the next meeting. She also noted that the Bylaws need some updates and they will be presented as well.
- 7.) As there were no further comments, Mr. Leeper made a motion to adjourn. Mr. Hartley seconded the motion. The motion was approved unanimously with 6 ayes (Adkins, Hartley, Larson, Leeper, Oberle, and Wingert), and 0 nays.

The meeting adjourned at 6:35 p.m.

Respectfully submitted,

Karen Howard

Community Services Manager

Joanne Goodrich Administrative Clerk

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Joanne Goodrick



DEPARTMENT OF COMMUNITY DEVELOPMENT

City of Cedar Falls 220 Clay Street Cedar Falls, Iowa 50613 Phone: 319-273-8600 Fax: 319-273-8610

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MEMORANDUM

Planning & Community Services Division

TO: Planning and Zoning Commission

FROM: David Sturch, Planner III

DATE: February 6, 2019

SUBJECT: Land Use Map Amendment

REQUEST: Land Use Map Amendment from Greenways and Floodplain to

Greenways/Floodplain and Planned Development (Case #LU19-001)

PETITIONER: Brent Dahlstrom, Owner; NewAldaya Lifescapes, Fehr Graham Engineering

LOCATION: 42.35 acre parcel at the southeast corner of W. 12th Street and Union Road

PROPOSAL

The petitioner has submitted a request to rezone 42.3 acres of land at the southeast corner of W. 12th Street and Union Road from A-1, Agricultural to R-P, Planned Residential. The rezoning will allow development of a NewAldaya Lifescapes campus that includes one and two unit dwellings and multi-unit dwellings for a 55 and over clientele. Zoning considerations normally involve evaluation of three main criteria:

- 1. Is the rezoning request consistent with the Future Land Use Map and the Comprehensive Plan?
- 2. Is the property readily accessible to sanitary sewer service?
- 3. Does the property have adequate roadway access?

This staff report will focus on the first criteria, the Future Land Use Map and Comprehensive Plan.

BACKGROUND

This property has been zoned as agricultural since adoption of the Zoning Ordinance in 1970. The land use designation is identified as Greenways/Floodplain. This designation was changed from agricultural/residential in the spring of 2012 as a part of the newly adopted Cedar Falls Comprehensive Plan. At that time, the majority of the property was located in the special flood hazard area (SFHA) or general floodplain and the greenway designation shown on the future land use map was intended to reflect the extent of the flood hazard area including a green

buffer. This buffer serves an important function for natural stormwater drainage and mitigates flood risk. In the spring of 2014, the floodplain through this property was re-mapped to reflect newer floodplain data when the upper reach of the University Branch of Dry Run Creek was studied. This study focused on the development of the southern portions of the Lexington Heights Addition and the creation of the stormwater detention facility north of the UNI Dome. Through improvements made to increase the flooding capacity and control the flow through the stream corridor, the flood hazard area on the subject property was significantly reduced, providing a larger area that is now developable. However, the Future Land Use Map was not amended at the time to reflect this reduction.

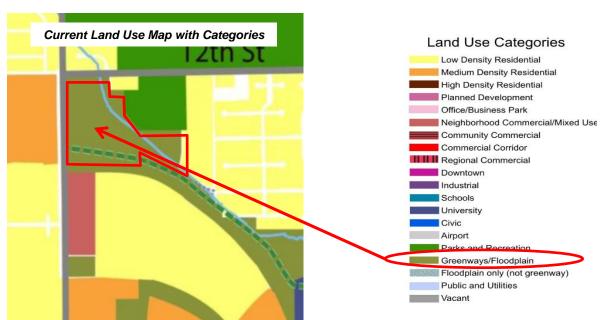
ANALYSIS

As stated in the Comprehensive Plan, land use is the central element of the Plan because it establishes the overall physical configuration of the city, including the mix and location of uses and community systems (utilities).

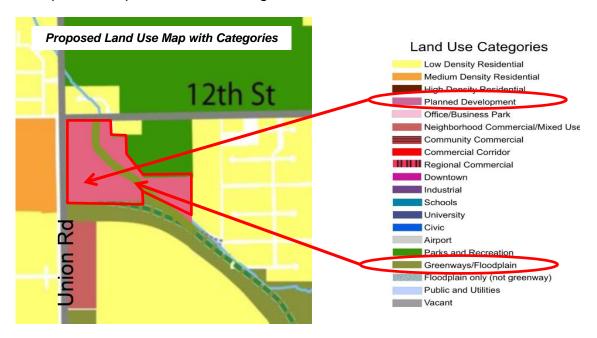
This 42-acre property is surrounded by residential development on the west, north and northeast. There is a single lot development to the south and the Robinson Dresser athletic complex to the east.

The land use analysis is the first step in the rezoning process. Before the property can be rezoned to allow residential development, the Future Land Use Map must be amended from "Greenways/Floodplain" to a combination of "Planned Development" and "Greenways/Floodplain" that will more closely distinguish the actual floodplain area from the developable portion of the property.

The greenways/floodplain category is identified in the Comprehensive Plan for traditional parks, recreational areas and environmentally sensitive areas to be possibly incorporated into the city's trail system. These areas are also reserved for stormwater management. A drainageway flows through the middle of the site in a northwesterly to southeasterly direction. The floodplain boundary buffers this drainageway but only encompasses a portion of the entire land area, which is approximately 13 acres. There is a considerable portion of the property that is developable.



The land use amendment corresponds to the proposed rezoning from A-1, agriculture to RP, planned residential. The concept development plan for this property includes the preservation of the floodplain, creating open space, trails, sidewalks and stormwater management areas for the neighborhood. One important feature of the stormwater management plan is to account for the additional stormwater that is received into this site from surrounding developments to the west and north. That stormwater, coupled with the stormwater generated on the site will be confined into detention basins and new storm sewers. Additional elements to this development include trails and sidewalks within and adjacent to the development. A city park is not planned for this property. There are other nearby parks surrounding this property including the Robinson Dresser Sports Complex, Pheasant Ridge Golf Course and Birdsall Park.



It seems appropriate to amend the future land use map to allow reasonable development of the property while still preserving and protecting the floodplain. The proposed amendment would maintain the location of the reduced greenway/floodplain and add the designation of "Planned Development" to the rest of the property

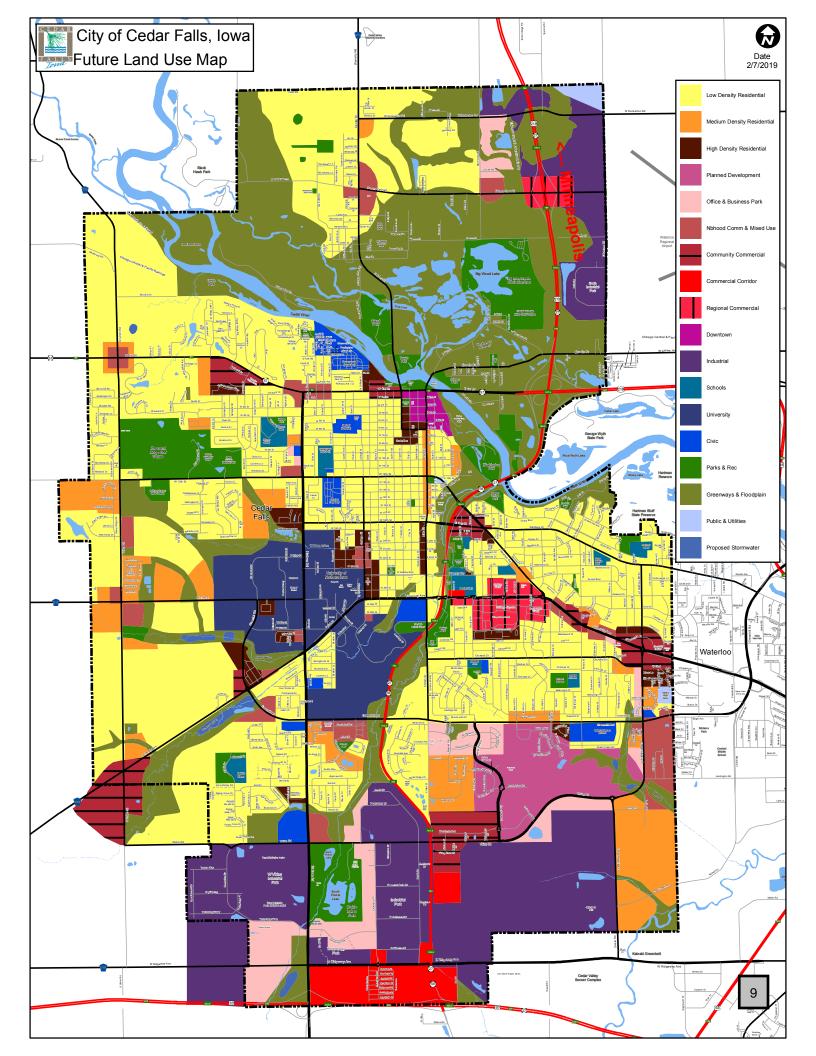
A notice was mailed to the adjoining property owners on February 6, 2019 regarding this request.

STAFF RECOMMENDATION

Gather any comments from the Planning and Zoning Commission and public then continue the discussion on this land use map amendment and set the date for public hearing for the next Planning and Zoning Commission meeting on February 27, 2019.

PLANNING & ZONING COMMISSION

Introduction 2/13/19





DEPARTMENT OF COMMUNITY DEVELOPMENT

City of Cedar Falls 220 Clay Street Cedar Falls, Iowa 50613 Phone: 319-273-8600 Fax: 319-273-8610

www.cedarfalls.com

MEMORANDUM

Planning & Community Services Division

TO: Planning and Zoning Commission

FROM: David Sturch, Planner III

DATE: February 6, 2019

SUBJECT: Rezoning Request

REQUEST: Rezone property from A-1, Agricultural to R-P, Planned Residence District

(Case #RZ19-001)

PETITIONER: Brent Dahlstrom, Owner; NewAldaya Lifescapes, Fehr Graham Engineering

LOCATION: 42.35 acre parcel at the southeast corner of W. 12th Street and Union Road

PROPOSAL

The petitioner has submitted a request to rezone 42.3 acres of land at the southeast corner of W. 12th Street and Union Road from A-1, Agricultural to R-P, Planned Residential. The rezoning will allow development of a NewAldaya Lifescapes campus that includes one and two unit dwellings and multi-unit dwellings for a 55 and over clientele.

The R-P district is guided by a development concept plan that the applicant submitted as part of this request. The development concept shows the general location of the streets, buildings, building lines, common areas, future floodplain and the storm water management areas. It is important to show the location of the streets and their extensions in order to create connections to other nearby existing and future neighborhoods. All the streets identified in the concept plan, except for the private drive for the multi-unit building at the northwest corner of the property, will be dedicated to the public and constructed according to city specifications. This plan will serve as a guide for the development of the preliminary and final plat that will be submitted at a later date.

The concept plan illustrates 69 one- and two-unit dwellings and two large multi-unit dwellings; one building with 40 to 50 units is located at the corner of W. 12th Street and Union Road and the second building with 20 to 25 units is located near the southwest corner of the site. It is proposed to establish 129 to 144 units on the 42 acres of land for a residential density of 3.07 to 3.42 units per acre. It should be noted that this is only a concept plan and the number of units is not guaranteed. The exact number of units will be determined during the platting and site plan

review process. All units will need to meet zoning and subdivision requirements, including stormwater management and floodplain regulations. At this residential density the development would be considered a low density residential development, but with the units clustered into some larger buildings in order to reserve more of the site as greenspace and to buffer the floodplain that extends through the property.

BACKGROUND

This property has been zoned as agricultural since adoption of the Zoning Ordinance in 1970. The land is surrounded by residential zoning on the west (across Union Road), north (across W. 12th Street) and northeast side of the property. The current owner is interested in selling this land to NewAldaya Lifescapes for a new 55 and over housing development.

The rezoning of this property must be carefully considered by evaluating the characteristics of the land and surrounding properties. This staff report will outline a number of these elements in order to have a firm understanding of the future use of this property.

ANALYSIS

The purpose of the A-1, agricultural zoning district is to act as a "holding zone" for future development when municipal services (sanitary sewer, water, roads) are accessible to the site. When these services are available, the development of the land is threefold, beginning with the rezoning of the land, platting and construction.

This 42-acre property is surrounded by residential development on the west, north and northeast. There is a single lot development to the south and the Robinson Dresser athletic complex to the east. This property is bisected with the western reach of the University branch of Dry Run Creek. A portion of the property is located in the 100-year floodplain which will need to be revised as part of the platting process. With the exception of the floodplain, there are no other sensitive areas within this rezoning plat.

Zoning considerations normally involve evaluation of three main criteria:

1) Is the rezoning request consistent with the Future Land Use Map and the Comprehensive Plan?

The current land use map is designated as greenways/floodplain. The proposed amendment will reduce the greenways/floodplain area and classify the remaining portion of the property to planned development. If the Planning and Zoning Commission agrees to the land use map amendment, as described in a separate staff report under Case # LU19-001, the proposed rezoning of this property can continue.

As part of the platting process, a stormwater management system will be designed to accept the stormwater runoff from the west and north. The stormwater will be directed toward multiple detention basins on the site and released at a controlled rate into the floodplain. The stormwater management system must be designed so that it will not adversely affect the current and future residents surrounding and within this development. Details of the stormwater management plan will be reviewed during the platting process to ensure that it meets all Code requirements.

2) Is the property readily accessible to sanitary sewer service? Yes, sanitary sewer is readily available to the site. This sewer is located through the middle of the property within the aforementioned floodplain and drainageway. This sanitary sewer is located in the Cherrywood Interceptor Sewer District which includes a sewer tapping fee as part of the development. The sewer tapping fee is \$294.63 per acre of development. This fee is paid by the developer at the time of final platting.

3) Does the property have adequate roadway access? The property currently has access from W. 12th Street and Union Road. The development concept plan shows two public street connections onto Union Road and one public street connection onto W. 12th Street. The two street connections onto Union Road are approximately 449' and 852' feet, respectively, south of W. 12th Street. The southern street connection onto Union Road is directly across from Sonoma Drive. The W. 12th Street connection is approximately 750' feet east of Union Road and it is directly across from Alexis Boulevard. The development concept plan also shows a street stub to the south and a street stub to the east. These street connections will be important for general traffic circulation and connections between neighborhoods and future neighborhoods. For example, while there may be no development planned for the existing large single family properties to the south, at some point in the future there may be a desire to further subdivide those lots. Without a planned street connection to this area, future infill development would not be possible. Staff notes that during the preliminary platting of the subject property, care should be taken to locate these street connections so they can be extended in a logical manner to adjacent properties and avoid conflicting with existing development. For example, the street connection to the south should be located to avoid extending too close to the existing home to the south. Angling this future roadway to the east to potentially parallel the creek may be an appropriate option.

The annual average daily traffic for this section of Union Road is approximately 3,000 vehicles per day. Depending on the circumstances and corridor constraints, a rural two-lane roadway could handle capacities up to 1,700 vehicles per hour during peak times. As developments more forward, City staff will continue to monitor traffic volumes along Union Road and when warranted, make improvements to Union Road as necessary.

A preliminary Developmental Procedures Agreement has been submitted for this rezoning request. The agreement describes the development details such as subdivision plats, description of proposed uses, maximum residential densities and agreement to submit all multi-unit developments for further site plan review by the Planning and Zoning Commission and City Council. The Development Agreement should be viewed as the narrative version of the graphic site plan. The agreement should also note that sanitary sewer and other utility connections will be extended to the site and that any sewer or water tapping fees will be paid at the time of platting and installation. The sewer service serving this area is subject to the Cherrywood Interceptor Sewer District tapping fee of \$294.63 per acre which was established by the City Council several years ago.

As part of the technical review of this proposal, Cedar Falls Utilities personnel, have no concerns with the proposed rezoning request. Water, electric, gas, and communication utilities are all available to this site from W. 12th Street and from Union Road per the service policy of each utility. There is a water connection fee of \$55,833.14 for this property based on the street lineal footage of W. 12th Street and Union Road. This connection fee is part of the cost of the original water main installations on W. 12th Street and on Union Road. All utility services will be extended into this property as part of the platting process.

A notice was mailed to the adjoining property owners on February 6, 2019 regarding this rezoning request.

STAFF RECOMMENDATION

Gather any comments from the Planning and Zoning Commission and public then continue the discussion on this rezoning and set the date for public hearing for the next Planning and Zoning Commission meeting on February 27, 2019.

PLANNING & ZONING COMMISSION

Introduction 1/23/19

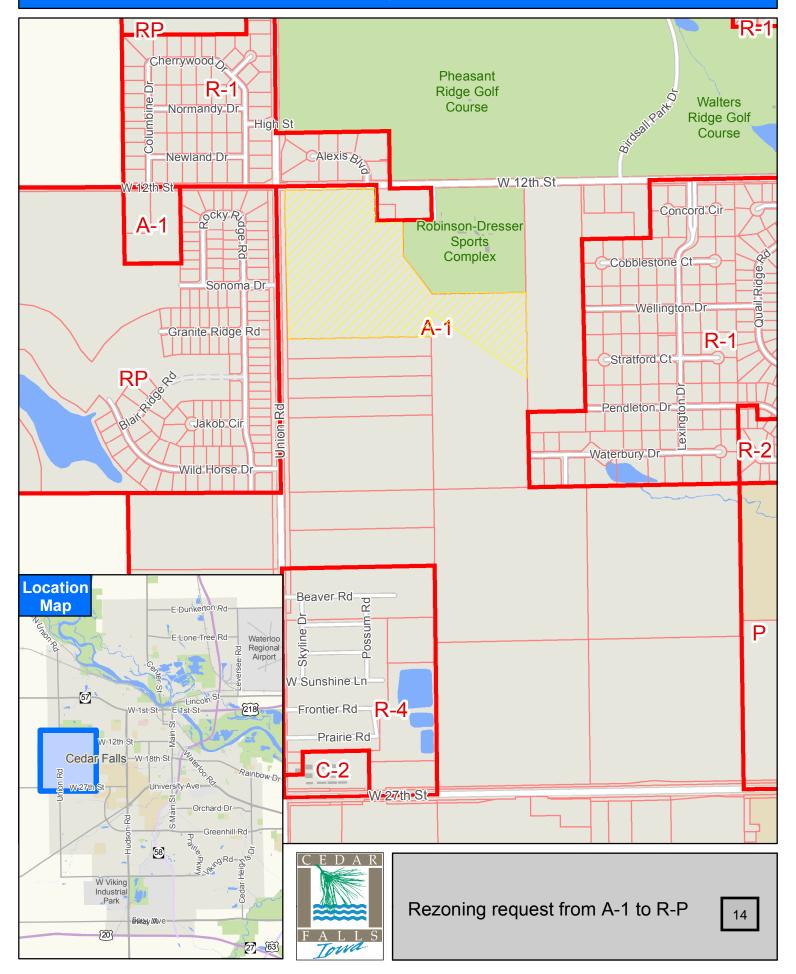
Chair Oberle introduced the land use and rezoning request. Mr. Sturch provided some background information. He explained that there is a request to rezone property 42.3 acres at the southeast corner of W. 12th Street and Union Road from A-1, Agriculture to RP, Planned Residential. It is proposed to build a 55 plus residential community for the NewAldaya campus with multi-unit dwellings. He discussed the concept plan for the development and the land use map amendment. Staff would like to discuss the item at this time and continue the discussion to the next meeting.

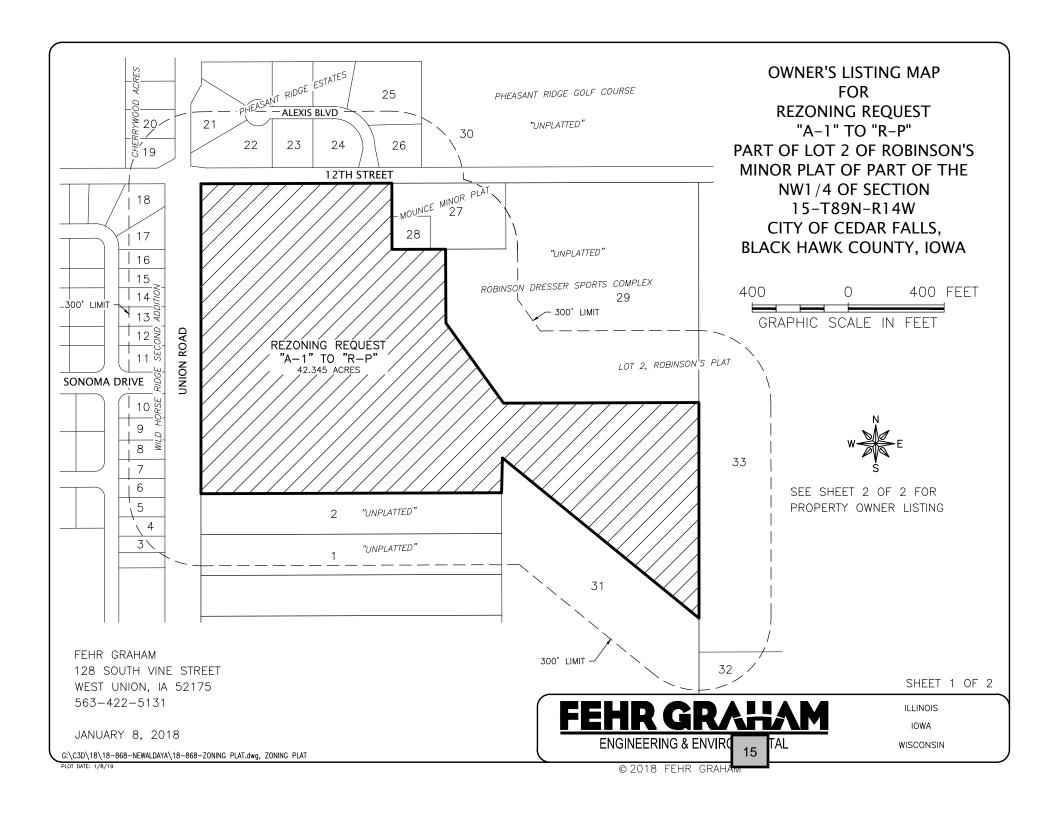
There were several neighbors in the audience that addressed the Commission. They expressed concerns with the existing floodplain boundary, stormwater management, runoff from the surrounding developments and golf course, traffic issues and proposed multi-unit structures.

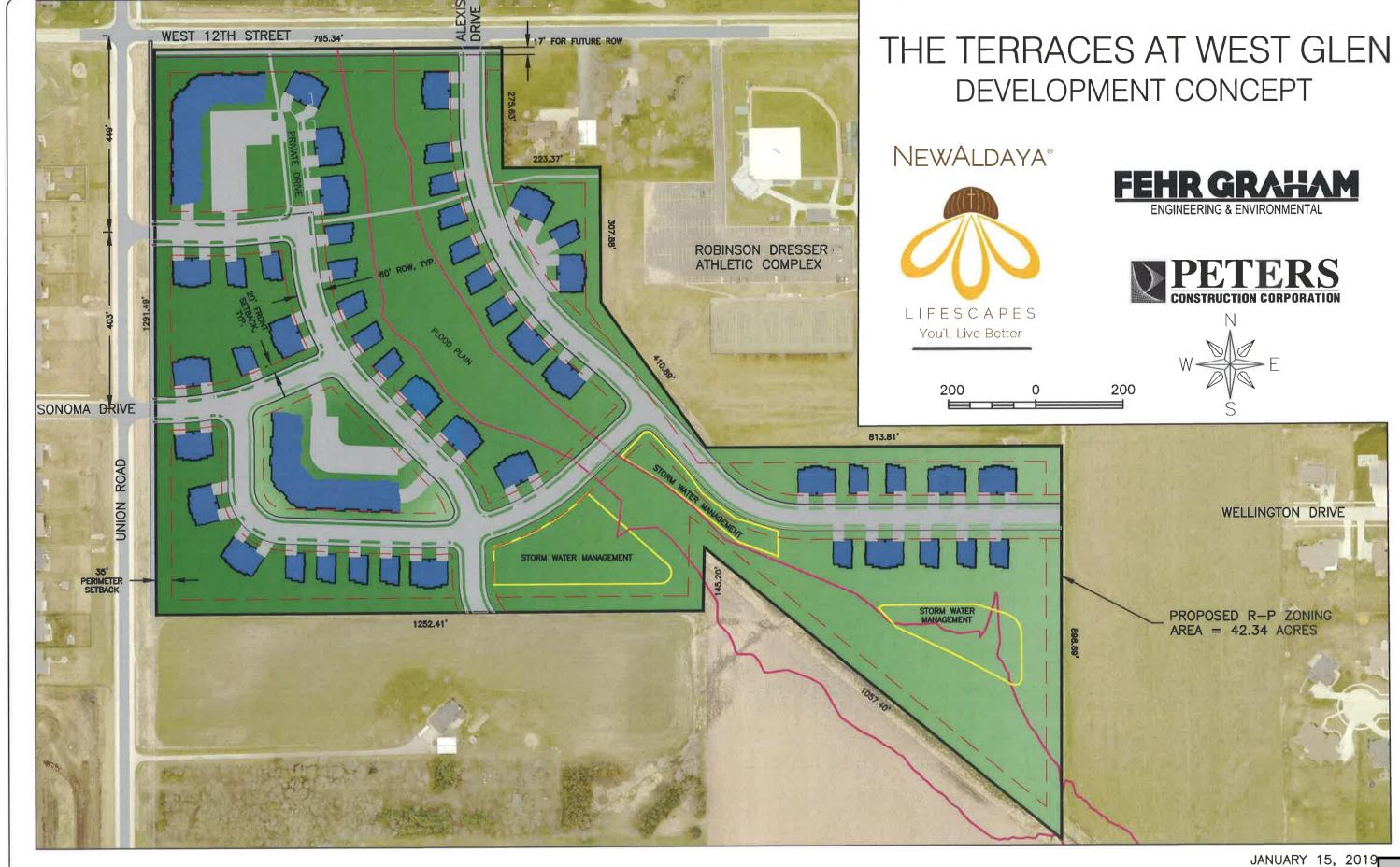
Mr. Wingert noted that he will be abstaining from this item. The Commission had some questions on the watershed and floodplain. The developer's engineer stated that they will review the floodplain and stormwater concerns with the platting phase. Their plan is to design the subdivision to accommodate the floodplain and stormwater concerns. The Commission also discussed the future roadway improvements because this area will change with additional development in western Cedar Falls. Staff will look into these concerns and set up a meeting with the developer and neighborhood before the next Commission meeting.

Discussion 2-13-19

Cedar Falls Planning & Zoning Commission February 13, 2019









DEPARTMENT OF COMMUNITY DEVELOPMENT

City of Cedar Falls 220 Clay Street Cedar Falls, Iowa 50613 Phone: 319-273-8600 Fax: 319-273-8610

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MEMORANDUM

Planning & Community Services Division

TO: Planning and Zoning Commission

FROM: Iris Lehmann, Planner II

Matt Tolan, Civil Engineer II

DATE: February 7, 2019

SUBJECT: Greenhill Village Townhomes II – Preliminary Plat

REQUEST: Request to approve the Greenhill Village Townhomes II Preliminary Plat

PETITIONER: Panther Farms LLC – owner; CGA Engineers – Civil Engineer

LOCATION: 5.3 acres southeast of the Greenhill Road and Hudson Road intersection

PROPOSAL

The applicant is proposing to subdivide a 5.3 acre parcel in the MU, Mixed Use Residential, zoning district with the intent to develop townhome buildings. The proposal consists of one developable lot, tracts for future streets and stormwater basins, and two outlots as placeholders for future development.

BACKGROUND

In 1998 the 130 acres of the Greenhill Village property was rezoned to MU, Mixed Use Residential. This rezoning was accompanied by the creation of a Greenhill Village Master Plan that was to be used as a guide for the development in this area. Since that time there have been a number of amendments to the Greenhill Village Master Plan. The most recent amendment to the Master Plan was approved in April 2018. This amendment to the Master Plan focused on the 5.3 acres southeast of the Greenhill Road and Hudson Road intersection; the area being considered in this request. The majority of the 5.3 acres were originally designated for singleunit residential development at a maximum density of 3.14 units an acre. In this particular section the Master Plan showed 12 single unit homes. The shaded area in green, in the image on the top right, was originally marked for commercial development. The approved amendment to the



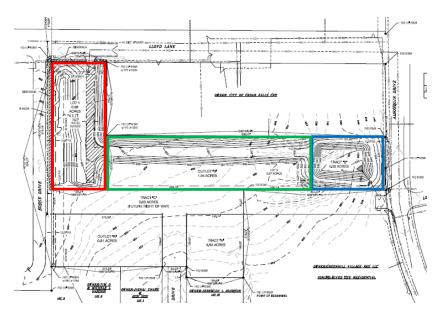


Master Plan allows for medium density, multi-unit, residential development at a maximum density of 7.7 units an acre. The concept plan that was approved with this update to the Greenhill Village Master Plan presented a development of townhomes, see image on previous page. The concept plan shows 40 townhome units.

ANALYSIS

The property in question is located within the MU, Mixed Use Residential, zoning district. The intent of the MU district is to encourage a variety of housing types and neighborhood commercial land uses for the purpose of creating viable, self-supporting neighborhood districts. Therefore, MU districts permit a variety of uses ranging from neighborhood commercial to office to single-unit homes to condominiums. The approved Greenhill Village Master Plan, attached, arranges the various permitted uses and densities by area and provides a guide for needed street connections within the district. The Master Plan designates this site for townhome development along an extension of Loren Drive, which will create an east-west connection to Norse Drive. The Master Plan also shows Addison Drive extending north to connect to the extension of Loren Drive. This connection will create another point of egress and ingress for the residential properties to the south. Additional street connections in this area will help to distribute traffic by providing multiple travel routes through the neighborhood. The layout of the proposed Greenhill Village Townhomes II Preliminary Plat provides the street connections as described and is consistent with the approved Greenhill Village Master Plan.

The proposed Preliminary Plat will create one buildable lot, Lot 1, outlined in red to the right. This lot will be 0.98 acres in size. The remainder of the site's 5.3 acres will be platted as a tract for future street connections (Tract C), outlots for future development (Outlots A and Outlot B), and two tracts for detention basins (Tract A and Tract B). The detention basin in Tract B, outlined in blue in the image to the right, will be installed with the creation of Lot 1. Along with the installation of this detention basin, the land in Outlot B. outlined in green, will be graded to guide stormwater from Lot 1 to this



detention basin. This improvement ensures that the water runoff from the new development that will occur on Lot 1 will be properly managed. This detention basin will be oversized to also serve future development that may occur in Outlot B. The second detention basin, Tract A, will be installed in the future when Outlots A and B are platted for development. A private alley is proposed coming south off of Lloyd Lane to provide access to Lot 1. This private alley connection meets city standards and allows more flexibility with the grading of the site as opposed to a connection coming from the south. Public sidewalks will be installed on the west and north sides of Lot 1 as well as the east side of Tract B. Other public sidewalks will be installed in conjunction with the future extension of Loren Drive and Addison Drive once Outlots A and B are platted for development. Utilities are readily available to this site. The developer will coordinate with CFU for extending the utility services to the proposed development.

Note that Outlots A and B may not be developed until a separate Preliminary and Final Plat are reviewed and approved by the Commission and the City Council.

Traffic Generation: At the last Commission meeting, concern was expressed about the amount of traffic that might be generated by this development. The City Engineer's Office notes that depending on the circumstances and corridor constraints, an urban two-lane roadway can handle capacities up to 1,000 vehicles per hour during peak times. The proposed subdivision shows a detention basin in the place of one of the six unit townhome buildings shown in the concept plan. With this change, the proposed development at full build-out could have 34 townhome units. A townhomes generate traffic similar to a single family home at approximately 7 trips per day per unit for a total of approximately 238 trips per day. With the new street connections proposed with this subdivision, multiple travel routes (Norse Drive, Lloyd Lane, Addison Drive, and the proposed Loren Drive) will be provided to nearby arterial and collector streets. Therefore, traffic generated by this development will not exceed the capacity of the existing streets. City staff will continue to monitor traffic volumes throughout the neighborhood and will make improvements as necessary.

TECHNICAL COMMENTS

City technical staff, including Cedar Falls Utilities (CFU) personnel, has reviewed the proposed plat. Technical comments are outlined below. The applicant resubmitted documents on Tuesday February 5, 2019. These updated documents are attached to this report. By the Planning & Zoning Commission meeting on February 13, staff will confirm if the technical items have been addressed.

- The list of record owners of property within 200' needs to include the proprietors of "Greenhill Village Fourth Condominiums".
- Adjoining record title lines should be called out in the legal description (the deed that the water tower sits on and the deed to the unplatted land to the South of proposed Tract "B").
- Add a public sidewalk on the east side of Tract "B".
- 4. A note about wetlands should be shown.
- 5. A utility easement needs to be shown of the North line of Lot 1 for the sanitary sewer.
- 6. The 10' PUE (assuming public utility easement) needs to be wider to accommodate the depth of the sanitary sewer on the West sides of Lot 1 and Outlot "A". Extend to 20 feet.
- 7. The boundary line of the addition needs to be a heavier line weight than the interior lot lines
- 8. On Sheet 2 of 2 remove "Lot 5 0.57 acres" language from the Plat
- 9. Deed of Dedication will be finalized with the Final Plat
- 10. Submit final stormwater management report at time of construction plans.

Basic platting documents have been submitted including plats and platting fee (\$300). A drafted Deed of Dedication, Attorney's Title Opinion, and Surveyor's Certificate have all been submitted.

The property is located outside of the designated floodplain. A courtesy notice to adjoining property owners for this preliminary plat was mailed on January 16, 2019. A second courtesy notice was sent to a larger area on February 4, 2019.

STAFF RECOMMENDATION

The proposed Greenhill Village Townhomes II Preliminary Plat is consistent with the zoning, the adopted master plan for this area, and meets the standards of the City's subdivision code,

provided minor technical issues are resolved. Therefore, Staff recommends approval of the proposed preliminary plat, subject to the resolution of the technical comments listed above. The preliminary plat is an intermediate step in the development process. Approval of a final plat and a site plan will be required before development can proceed.

PLANNING & ZONING COMMISSION

Discussion 1/23/2019

Chair Oberle introduced the item and Ms. Lehmann provided background information. She explained that Panther Farms LLC is proposing to subdivide 5.3 acres of land southeast of the Greenhill and Hudson Road intersection. She displayed the approved concept plan tied to this location and explained that the area is considered medium density residential. The proposed Subdivision Plat is consistent with the concept plan. The proposal consists of one developable lot (Lot 1), tracts for future streets and stormwater basins, and two outlots as placeholders for future development. She discussed the proposed detention basin and the flow of the runoff, sidewalks, and a private alley that will service Lot 1. Utilities are available to the site. Ms. Lehmann noted that another Preliminary and Final Plat will be required before the two outlots can be developed. Staff recommends gathering comments from the Commission at this time and continuing discussion at the next Planning and Zoning meeting.

Mr. Wingert recused himself due to a conflict of interest.

James Denny, 4622 Hudson Road, stated that the area was supposed to be buffered with single-family houses and explained his concerns with traffic and safety issues that this higher density development would bring. He claimed to have been unaware of the Master Plan update that occurred in the spring of last vear.

Tim Hanson, 1517 Athens Court, noted his concerns of decreasing property values and water runoff issues. He also noted that this was the first time he was made aware of higher density development being allowed in this area.

Ms. Lehmann noted that staff has looked into the water runoff issues and noted that at full-build-out there will be two detention basins that will serve the site. The site will be graded so that stormwater will flow toward the new stormwater basins and not on to neighboring properties.

Mr. Leeper asked if traffic concerns had been looked into. Ms. Lehmann said that staff would look into it.

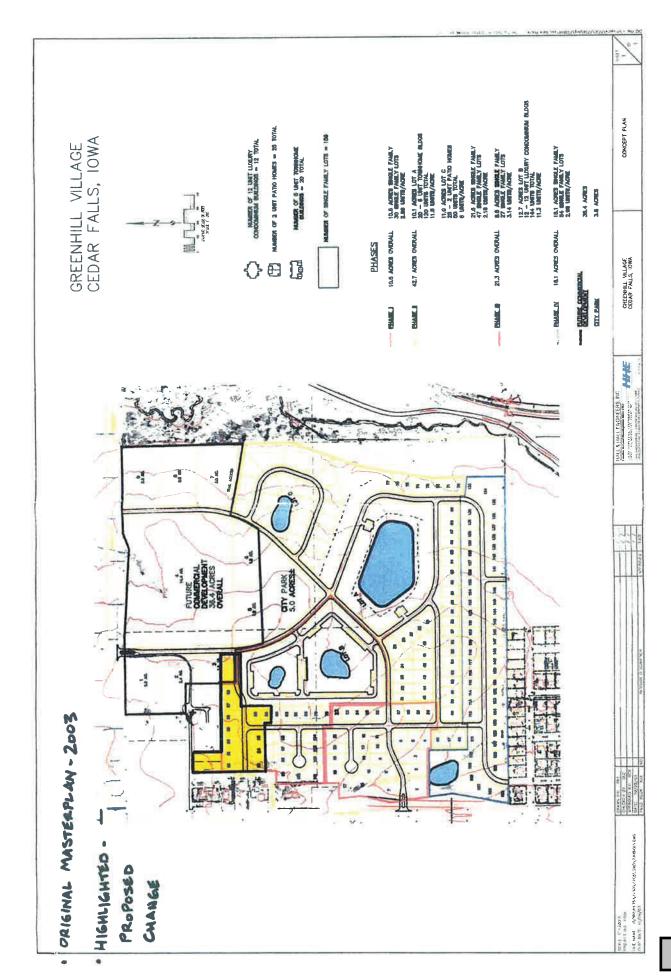
The item will continue to the next meeting.

Discussion 2/13/2019

Attachments: Greenhill Village Master Plan

Proposed Preliminary Plat (updated) Drafted Deed of Dedication (updated)

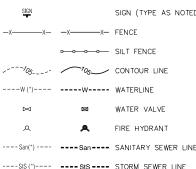
Written correspondence



PRELIMINARY PLAT GREENHILL VILLAGE TOWNHOMES II

CEDAR FALLS, IOWA

LEGEND PROPOSED EXISTING EVERGREEN TREE DECIDUOUS TREE (SB) (SB) SHRUBS (BUSHES) \sim TREE LINE SIGN (TYPE AS NOTED) -X- FENCE







-----G----- GAS LINE

 \oplus

 $\langle \overline{1} \rangle$

-OHE- OVERHEAD ELECTRICAL LINE

GAS VALVE

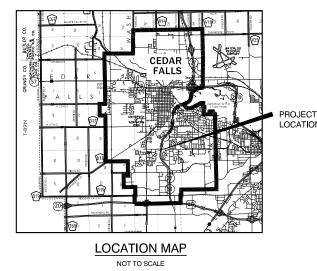
----E(*)---- BURIED ELECTRICAL LINE

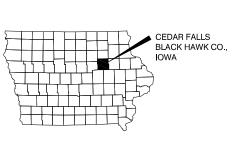
POWER POLE STREET LIGHT

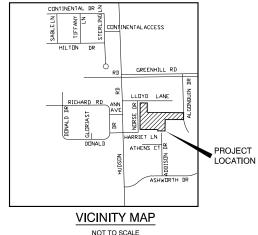
FLECTRICAL

----T---- TELEPHONE LINE

TELEPHONE PEDESTAL







LEGAL DESCRIPTION

SAID PROPERTY IS LEGALLY DESCRIBED AS:

GREENHILL VILLAGE TOWNHOMES II LOCATED IN THE NORTHWEST QUARTER OF THE NORTHEAST QUARTER OF SECTION 26, TOWNSHIP 89 NORTH, RANGE 14 WEST OF THE FIFTH PRINCIPAL MERIDIAN, CITY OF CEDAR FALLS, BLACK HAWK COUNTY, IOWA. MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF A CERTAIN PARCEL OF LAND DESCRIBED AS LOT 12 OF GREENHILL VILLAGE SECOND ADDITION AND RECORDED IN INSTRUMENT NO. 2005-00020608 IN THE OFFICE OF THE RECORDER BLACK HAWK COUNTY, IOWA; THENCE, N89°39'54"W 139.92' ALONG THE NORTH LINE OF SAID GREENHILL VILLAGE; THENCE, N0°19'47"E 26.06'; THENCE, N89°55'09"W 60.00'; THENCE, CONTINUING N89°55'09"W 273.33' TO A NORTHWEST CORNER OF LOT 3 OF SAID GREENHILL VILLAGE, THE AFORESAID ALL BEING ALONG THE NORTH LINE OF SAID GREENHILL VILLAGE, SAID POINT ALSO BEING A POINT ON THE EAST RIGHT OF WAY LINE OF NORSE DRIVE THENCE, N0°00'16"W 504 72' ALONG SAID EAST RIGHT OF WAY LINE TO A POINT ON THE SOLITH RIGHT OF WAY LINE OF LLOYD LANE; THENCE, S89°37'56"E 135.97' ALONG SAID SOUTH RIGHT OF WAY LINE; THENCE, S0°08'01"W 180.13", THENCE, S89°35'24"E 688.47" TO A POINT ON THE WEST RIGHT OF WAY LINE OF ALGONQUIN DRIVE; THENCE, S0°09'23"W 76.99' ALONG SAID WEST RIGHT OF WAY LINE, THENCE, SOUTHERLY 53.45' ALONG THE ARC OF A 780.00' RADIUS CURVE CONCAVE EASTERLY. HAVING A CHORD BEARING OF \$1°47'42' E AND A CHORD DISTANCE OF 53.44' ALONG SAID WEST RIGHT OF WAY LINE; THENCE, N89°55'11"W 351.07; THENCE, S0°20'02"W 216.23' TO THE POINT OF BEGINNING, CONTAINING 5.21 ACRES. SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY

I FURTHER CERTIFY THAT THE ACCOMPANYING PLAT IS A TRUE REPRESENTATION OF SAID PROPERTY IN ACCORDANCE WITH MY FIELD NOTES; THAT THE DIMENSIONS OF THE STREETS, LOTS, AND EASEMENTS DEPICTED ON SAID PLAT ARE IN FEET AND DECIMALS THEREOF; AND THAT THIS LAND SURVEYING DOCUMENT WAS PREPARED AND RELATED SURVEY WORK WAS PERFORMED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL LAND SURVEYOR UNDER THE LAWS OF THE STATE OF IOWA.

SURVEYOR AND ENGINEER

JEREMY A. HARRIS, P.L.S. ADAM DATERS, P.E. CLAPSADDLE-GARBER **ASSOCIATES** P.O. BOX 754 - 16 E. MAIN MARSHALLTOWN, IOWA 50158

ZONING INFORMATION:

CURRENT: MU - MIXED USE

SURVEY REQUESTED BY:

PANTHER FARMS, L.L.C. BRIAN WINGERT CEDAR FALLS, IOWA 50613

CLOSURE:

- ALL SUBDIVISION BOUNDARIES ARE WITHIN THE 1:10,000 ERROR OF CLOSURE REQUIREMENT - ALL LOTS ARE WITHIN THE 1:5000 ERROR OF CLOSURE

NOTE:

ALL BEARINGS ARE THE RESULT OF G.P.S. OBSERVATIONS USING NAD83 IOWA STATE PLAN NORTH ZONE

OWNERS OF RECORD

PANTHER FARMS LLC 604 CLAY ST. CEDAR FALLS, IOWA 50613

FLOOD ZONE:

FEMA FIRM MAP NUMBER 19013C0277F ZONE X (UNSHADED) EFFECTIVE JULY 18, 2011.

SITE DATA:

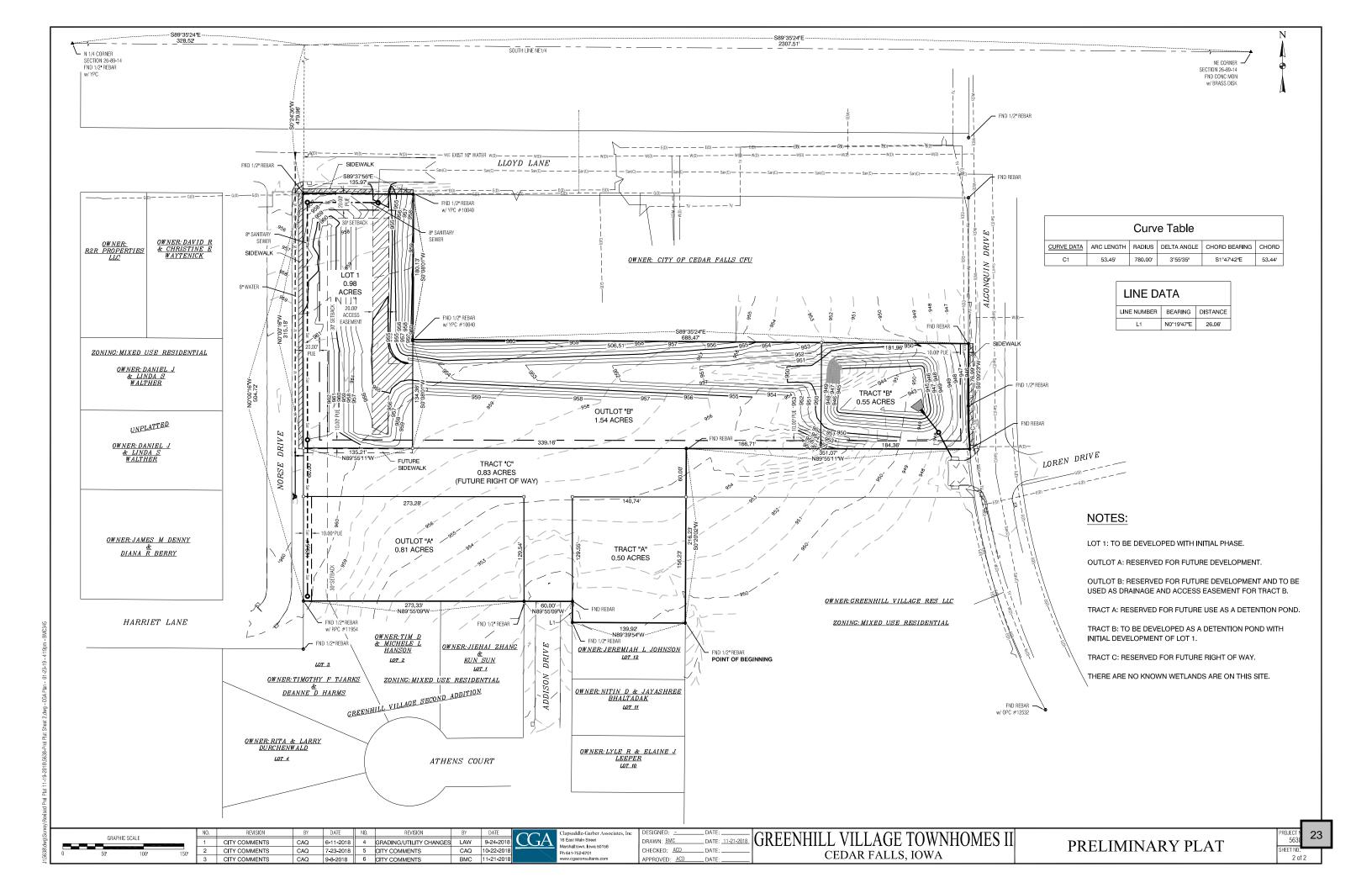
SETBACK=30' (CODE ALLOWS FOR 20' w/ APPROVAL)

SURVEY LEGEND

- ▲ GOVERNMENT CORNER MONUMENT FOUND
- A GOVERNMENT CORNER MONUMENT SET 1/2" x 30" REBAR w/BLUE PLASTIC ID CAP #22259
- PARCEL OR LOT CORNER MONUMENT FOUND
- o SET 1/2" x 30" REBAR w/BLUE PLASTIC
- () RECORDED AS







DEED OF DEDICATION OF GREENHILL VILLAGE TOWNHOMES II CITY OF CEDAR FALLS, IOWA

KNOW ALL MEN BY THESE PRESENTS:

That Panther Builders, LLC, an Iowa limited liability company, with its principal office in Cedar Falls, Iowa; being desirous of setting out and platting into lots and streets the land described in the attached Certificate of Survey by Travis R. Stewart, a Professional Engineer and Licensed Land Surveyor, dated _____ day of ______, 20198, do by these presents designate and set apart the aforesaid premises as a subdivision of the City of Cedar Falls, Iowa the same to be known as:

GREENHILL VILLAGE TOWNHOMES II CITY OF CEDAR FALLS, IOWA

("Development") all of which is with the free consent and the desire of the undersigned and the undersigned do hereby designate and set apart for public use the street(s) as shown upon the attached plat.

EASEMENTS

The undersigned do hereby grant and convey to the City of Cedar Falls, Iowa, its successors and assigns, and to any private corporation, firm or person furnishing utilities for the transmission and/or distribution of water, sanitary sewer, gas, electricity, communication service or cable television, perpetual easements for the erection, laying building and maintenance of said services over, across, on and/or under the property as shown on the attached plat.

RESTRICTIONS

Be it also known that the undersigned do hereby covenant and agree for themselves and their successors and assigns that each and all of the residential lots in Development be and the same are hereby made subject to the following restrictions upon their use and occupancy as fully and effectively to all intents and purposes as if the same were contained and set forth in each deed of conveyance or mortgage that the undersigned or their successors in interest may hereinafter make for any of said lots and that such restrictions shall run with the land and with each individual lot thereof for the length of time and in all particulars hereinafter stated, to-wit:

- 1. ____Any building that shall be erected shall have a minimum setback from the front, side, and rear of the lot lines as indicated on attached Final Plat. All minimum setbacks will be required to meet or exceed M-U Mixed Use Zoning.
 - 2. ___Only buildings containing townhomes shall be constructed on Lot 1s.

- 3. _____Tract_Outlot "A"-to be deeded to the City of Cedar Falls, Iowa for public street purposes. That portion of Tract "A" south of Lot 5 and Tract C Outlot A will not be required to be constructed until such time as Lot 5 Outlot C is developed, with the costs of construction to be shared between Developer the owner of Outlot C and the owner of the real estate to the south of Tract "A".Outlot B.

 4. Tractss "BA" & "BC" shall be used for water retention and shall be maintained by
- 4. ___Tractss "BA" & "BC" shall be used for water retention and shall be maintained by the Greenhill Village Townhomes II of Cedar Falls Owners Association. _-The City of Cedar Falls and the Greenhill Village Townhomes II of Cedar Falls Owners Association shall have access to said detention ponds for maintenance pursuant to the Maintenance and Repair Agreement and Permanent Easement between Developer and the City of Cedar Falls.
- 5. ___All private streets in Development shall be maintained by the Greenhill Village Townhomes II of Cedar Falls Owners Association.
- 6. ___Any and all drainage easements will be required to follow the "Stormwater Management Plan" and no building structures, fence structures, landscaping structures, private gardens or any other possible obstruction can be built in and over said drainage easements. All lot owners and/or contractors working on said lots will be responsible to maintain said easements to be free and clear of any physical obstruction(s) thus allowing the conveyance of overland storm water runoff as intended per "Stormwater Management Plan" on record with the City of Cedar Falls Engineer's Office.
- 7. ____The Owner and/or occupant of each Lot shall jointly and severally be responsible to keep in good order or to maintain the area between the curbline and the property line abutting their property including keeping said area free of holes, pitfalls, stumps of trees, fences, brick, stone, cement, stakes, posts or rods to which a metal, plastic or similar receptacle designed to hold newspapers are affixed, private irrigation or sprinkler systems, retaining walls, landscaping brick, block, stone, timber or other similar materials, or any other simulator obstruction. All mailboxes shall be clustered or grouped for the units, and shall not be placed between the curb line and the property line abutting the lots.
- 8. ____All townhomes shall be subject to the Declaration of Submission of Property to Horizontal Property Regime for Greenhill Village Townhomes II Cedar Falls, Black Hawk County, Iowa to be filed within one (1) week of the filing of the Final Plat.
- 9. ____Every lot in Addition and the owner thereof shall be subject to all of the provisions of the Greenhill Village Master Plan currently on file with the City of Cedar Falls, Iowa, at the time of construction and the lot, and all amendments thereto, including but not limited to the design guidelines contained therein.
- 10. The undersigned and all persons and entities hereafter acquiring any right, title, or interest in any of the Lots in said Development shall be taken and held to have agreed and covenanted with the owners of all other Lots in this Development and with the respective successors and assigns of all of the rest of such other Lots to conform to and observe all of the foregoing covenants, restrictions, and stipulations as to the construction of building thereon, for a period of 21 years from the date of filing of said plat, and this deed of dedication for record.

Within the period of 21 years and in accordance with Iowa Code § 614.24 and § 614.25 or their successor provisions, these covenants, restrictions, and stipulations may be extended for an additional period of 21 years upon compliance with § 614.24 and § 614.25 of the Code of Iowa. In the event an extension of the covenants, restrictions, and stipulations is not filed within the period of 21 years or successive 21-year periods, then the covenants, restrictions, and stipulations contained herein shall terminate at the end of the existing period of 21 years.

- 11. Invalidation of any of these covenants by judgment, decree, or court order, shall in no way affect any of the other provisions of this dedication and such other provisions shall remain in full force and effect.
- 12. If the parties hereto, or any of them, or their heirs or assigns shall violate or attempt to violate any of the covenants or restrictions here, it shall be lawful for any other person owning property in said addition to prosecute any proceedings at law or in equity against the person or persons violating or attempting to violate any such covenants or restrictions, and for the purpose of preventing such acts or recovering damages for such violations or both, and for costs and reasonable attorney fees as determined by the court.

PUBLIC IMPROVEMENTS REQUIRED IN PLAT

- 1. The public street(s) shown on the attached plat, will be brought to City grade and that the street will be thirty-one (31) feet, back of curb to back of curb, with approved hard surface pavement in accordance with the City of Cedar Falls, Standard Specifications unless otherwise specified as per approved construction plans.
- 2. Sanitary sewer, together with the necessary manholes and sewer service lines to all buildings in the plat will be provided.
- 3. That underground utilities, as required by the Subdivision Ordinance of the City of Cedar Falls, Iowa, shall be installed.
- 4. That city water will be provided to all buildings as required by the Cedar Falls Municipal utilities.
- 5. That municipal fire hydrant(s) will be provided as required by the Cedar Falls Public Safety Department.
 - 6. That Storm sewer will be provided as specified by the City Engineer.
 - 7. That handicap ramps will be provided as required by law.
- 8. All buildings erected on any lot in this Development shall be constructed in accordance with the building, plumbing and electrical codes of the City of Cedar Falls.
- 9. The Developer or its successors will install a 4-foot wide concrete sidewalk four inches thick across the entire frontage of any Lot, and Side of the Lot, on and corner Lots, at the

time of construction upon said Lot. This shall include handicap ramps as provided by state law. Any Lots remaining vacant for five (5) years after the date of final approval of the plat, shall be improved with sidewalks as soon as the construction season permits.

- 10. That the work improvements called herein shall be in accordance with the specifications of the City of Cedar Falls, Iowa, and performed under the supervision of the City Engineer. In the event that the developer, Panther Farms, LLC, it grantees and assigns fail to complete said work and improvements called for within one (1) year from the date of the acceptance of said final plat by the City of Cedar Falls, Iowa, the City may then make improvements and assess the costs of the same to the respective lots. The undersigned, for themselves, their successors, grantees and assigns, waive all statutory requirements of notice of time and place of hearing and agree that the City may install said improvements and assess the total costs thereof against the respective lots.
- 11. That the City may perform said work, levy the cost thereof as assessments, and the undersigned agree that said assessments so levied shall be a lien on the respective lots with the same force and effect as though all legal provisions pertaining to the levy of such special assessments have been observed, and further authorize the City Clerk to certify such assessments to the County Auditor as assessments to be paid in installments as provided by law.
- 12. The Developer shall construct and install all required public improvements within the subdivision plat, to conform with approved construction plans which meet the specifications of the City of Cedar Falls, Iowa. Such required public improvements shall meet the following requirements:
 - A. Shall be constructed and installed in a good and workmanlike manner;
 - B. Shall be free of defects in workmanship or materials;
 - C. Shall be free of any conditions that could result in structural or other failure of said improvements;
 - D. Shall be constructed and installed in accordance with the design standards and technical standards established for such public improvements by the City and by Cedar Falls Utilities;
 - E. Shall be constructed and installed in strict compliance with the minimum acceptable specifications for the construction of public improvements set forth in the Cedar Falls Code of Ordinances, including without limitation, Chapter 24, Subdivisions, and as such specifications shall be recommended for approval by the City Engineer from time to time, and approved by the city council.
- 13. The Developer's construction plans are now on file in the Office of the City Engineer.

SIGNED and DATED this day of, 201	9	<u>)</u>	ł	ð	ð	è	5
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	Panther Builders, LLC
	Brent Dahlstrom, Manager
STATE OF IOWA, BLACK HAW	K COUNTY: ss
Public in and for the State of Iowa, Builders, LLC, to me known as the	, 20198, before me, the undersigned, a Notary personally appeared Brent Dahlstrom, Manager of Panther identical persons named in and who executed the foregoing they executed the same as their voluntary act and deed on
	Notary Public in and for the State of Iowa

Dear Planning and Zoning Committee,

My name is Michele Hanson, my family and I have lived at 1517 Athens Ct. in Greenhill Village for nine years. My children have grown up here, rode their bikes through the neighborhood, and we have enjoyed living here with our neighbors, so you can imagine our shock and dismay at the note we received January 17th inviting us to a meeting with the Planning & Zoning on January 23rd. Not only was this short notice but this letter was only sent out to only a small handful of neighbors with many that this will affect not receiving this letter at all. The big shocker in all this is that the notice was to let us know about a meeting scheduled to discuss the plan for the area directly behind our house to be developed with townhomes! This is a huge punch in the gut shocker because this land has always been in the plans to be developed into more "single family" homes! My husband is the president of our HOA and at NO time did he, the HOA, nor anyone in this neighborhood received any notice of any meetings prior to this stating that these plans had changed and that this land had been rezoned from single family homes to multifamily (Townhouse/apartments). In doing our research, and with the help of Shane Graham it was brought to our neighborhood associations attention that meetings were held on March 28 (P & Z) to ask for this property to be amended with density of townhomes and then on April 16 (City Council resolution #21,071) which approved this request! These meetings were held WITHOUT any of us in Greenhill Village ever being notified! I believe this takes away our rights to attend and share our thoughts on what happens in "OUR" neighborhood!

From the meeting minutes: (Mr. Holst verified that notifications were sent to the neighbors.)

Did it not seem strange to all involved that not one homeowner from GV was there to object to this idea or share their concerns? That's because you took away our rights which we will share with the lawyer we have retain this to represent Greenhill Village in this matter.

We ask for your help and fairness in this matter!

I sent out a questioner on our GV website to the neighbors that were supposedly sent the notice back on 3/19/18 and NOT ONE received this letter! It is highly unlikely that this many letters just happen to be lost in the mail. These notices were never sent! We are asking that the amendment made to this property from single family homes to multifamily units on April 16, when it was unanimously voted upon be the city council again without any notice to GV families be reversed!

Send out notices this time and follow the correct protocol that is put in place for a reason that allows homeowners to express their concerns when it comes to their neighborhood!

These are some of the concerns that our HOA and surrounding neighbors have with this plan:

- DENSITY of multi-family units vs single family homes in this neighborhood. We have seen the plans of 40 units / 8 complexes in the area starting with the two in the northwest corner.
- TRAFFIC getting onto Greenhill Road has been a concern for some time and is near impossible at Ashworth and Hudson turning south. Many neighbors choose to drive to Erik and Hudson which increases the neighborhood traffic and that will only be increased with Addison being extended and traffic going from Harriet and Hudson turning south.
- RUNOFF while yes you have plans for a retention pond, our neighborhood ponds have overflowed this past year alone due to heavy rains and what are the plans for this pond? It will flood and run into the neighboring yards of those who's backyards are on Addison and Ashworth and this area is a wet land area that already has standing water in it at any given time.
- BUFFER/PRIVACY the area that is in the plans to be densified by these multifamily homes (40 units/8 complexes) is an extreme density in one area without sufficient buffer between single family homes and multi-unit buildings. The fact that 3 story townhouse will be looking into our ranch home is an invasion of our privacy! The fact that the driveway to one of these units will be directly perpendicular to our yard which means that the lights from the vehicles will be shining directly into our house!
- PROPERTY VALUE we all know this will bring revenue through taxes but what about the value of our homes? We all know this answer!

Respectfully,

Michele Hanson

From: Robin Frost <frostrr@gmail.com>
Sent: Monday, January 28, 2019 8:39 PM

To: Iris Lehmann

Subject: Greenhill Village Townhomes II

Follow Up Flag: Follow up Completed

Dear Ms. Lehmann,

I am writing to express my concern at the total lack of communication the Greenhill Village neighborhood has received about the change in the bordering property development. As residents of the Greenhill Village neighborhood, we have not received ANY communication from the City of Cedar Falls about the amendment to the master plan and proposed development adjacent to our homes. The first my husband and I learned of the issue was a Facebook post by a fellow resident to the Greenhill Village Facebook Group on January 23 at 4:54 p.m., approximately one hour prior to the Planning & Zoning Committee meeting. As I did not see the message until much later, I was unable to attend the meeting that evening.

Upon reviewing the memorandum in the Community Development packet, I was disturbed to learn there had been an amendment to the Greenhill Village master plan last April (2018) to allow the area to be developed as townhomes rather than the original intention of single-family homes. I was further disturbed that neither my household, nor any of the neighbors, had received communication about the meeting or potential change prior to last April's decision. I have heard indirectly that the city's records show it sent notices about such a meeting. However, my own records (along with all of my neighbors') show that no such letter was received.

I assure you that as a new resident to Greenhill Village (as of December 2017) I was hyper-aware that development may be possible nearby, so I would have been vigilant about any communication that came from the city about this subject.

Furthermore, we did not receive a notice of the Planning & Zoning meeting on Jan. 23 where the proposed development was discussed, nor was the Greenhill Village Homeowners' Association notified. If the Homeowners' Association had received notice, the subject certainly would have been on the agenda for the annual HOA meeting which took place on Jan. 21, just two days prior to the Planning & Zoning meeting.

Not only am I dismayed at the total lack of communication from the City, but also at the proposed plan and its rapid progression through the development process. It is disturbing to read a plan that the neighborhood had no part in developing or influencing, and realize how far along the proposed development is in the process. As I read the recommendations and technical comments from the City technical staff and CFU, it appears that commencing development is imminent and inevitable. I am appalled that the City would allow this process to continue without following the proper channels, and with the knowledge that none of the neighborhood residents received the required written communication.

As you may know, adding rental units to a neighborhood can reduce nearby property values by 13.8% (American Community Survey) by reducing the desirability of the neighborhood due to safety and aesthetic concerns. Parking, water runoff, sewage and garbage are additional concerns, several of ware not addressed in the development request. Additionally, the City will want to consider the flood

that occurred in the area on Labor Day 2018 when planning for drainage and runoff. Addison Drive/Athens Court had several feet of water, which impacted the basements of many nearby homes causing property damage (inside dwellings and to outdoor landscaping), insurance claims, and complaints to the city. As this flood event had not yet occurred when the amendment was made to the plan, surely this incident should be a factor for consideration and discussion by the city and any future developer.

I am proud to live in Cedar Falls, and specifically in Greenhill Village, but disappointed by the manner in which this issue is being handled. I respectfully request a delay in this development request in order to revisit the amendment which was made without the opportunity for neighborhood residents to weigh in, research the impact and discuss with our neighbors.

Thank you for your consideration,

Robin Frost 4718 Addison Drive Cedar Falls

From: Vinod Phuke <vinodphuke@gmail.com>
Sent: Friday, February 01, 2019 11:34 AM

To: Iris Lehmann

Subject: Raising concern 8-4 Town homes recently approved by planning and zoning

department

Follow Up Flag: Follow up Flag Status: Flagged

Hello Iris,

I would like to raise my concern related to 8-4 Town homes recently approved by planning and zoning department. This was originally planned for the single family homes, this change will negatively impact on living since these town homes will be rented to tenants and we are currently facing lots of traffic and parking issues, this new plan will make things worse as a member of Greenhill village family I would like to raise my concern and strongly oppose this new plan

Best regards

Vinod Phuke

1029 Amelia Dr Unit 1

Cedar Falls IA-50613

319-610-8880

From: Larry Durchenwald <ldurch@cfu.net>

Sent: Tuesday, February 05, 2019 9:54 AM

To: Iris Lehmann

Subject: New townhouses at Greenhill Village

Members of Cedar Falls City Council;

My name is Larry Durchenwald, and my wife and I are current residents of 1525 Athens Ct. We are retired and have been part of the neighborhood since June 2013. We enjoy the neighborhood and have gotten to appreciate knowing our neighbors.

We are against the new townhouses in the area of Norse Drive, because that intersection onto Hudson Road is very busy as it is, and additional, high density, occupancy, dwellings will create a traffic issue trying to enter Hudson Road. Often, there is a line of cars waiting for the traffic light at the intersection of Greenhill Road and Hudson road, so entering Hudson Road during those situations will be very frustrating.

It is not unusual to see cars going around that tight corner of Harriet Lane and Norse Drive on the inside of the turn, no matter what direction they are going. Visibility in this corner is limited at best.

If this area is to be developed, we much prefer the addition of single family housing where properties will be better cared for by actual property owners, rather than temporary renters. We feel, also, that townhouses this close to our established neighborhood, will decrease the value of our homes.

Larry & Rita Durchenwald

1525 Athens Ct.

Cedar Falls, Iowa 50613

From: Iris Lehmann

Sent: Monday, February 04, 2019 8:15 AM

To: 'Karmen Woelber'

Subject: RE: Greenhill Village area zoning

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Good morning Karmen,

Thank you for your email. I just want to assure you that a zoning change for apartment buildings has not been approved for this area nor is it being considered. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP

Planner II

City of Cedar Falls, Iowa Phone: 319.268.5185

From: Karmen Woelber [mailto:karmen@cfu.net]

Sent: Friday, February 01, 2019 9:15 PM

To: Iris Lehmann

Subject: Greenhill Village area zoning

Ms. Lehmann,

I live at 4808 Algonquin Dr. Unit 6. I have just been informed through my homeowner's association that there are plans to change the zoning in the area in front of the water tower. Plans to change it from single family dwelling zoning to apartment zoning. I have significant concerns about the negative impact this change would have on our neighborhood and property values. I have also been told that the city states all residents in the area have been previously informed of the anticipated change. I want to state very clearly that I have **not** received any information at all regarding this issue. The first I new of it was from an email via my homeowner's association that I received today.

Respectfully yours,

Karmen Woelber

From: Iris Lehmann

Sent: Monday, February 04, 2019 8:19 AM

To: 'Dad' Subject: RE:

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Good morning Dean,

Thank you for your email. I just want to assure you that plans for "massive rental units" are not being considered. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP Planner II City of Cedar Falls, Iowa Phone: 319.268.5185

From: Dad [mailto:dlb51111@juno.com]
Sent: Friday, February 01, 2019 5:09 PM

To: Iris Lehmann

Subject:

Iris, I have just been informed of the proposed building of massive rental units close to where i live. The City of CF is claiming all homeowners close to that area were notified, This is false. I was never notified of such a proposition and am completely against such. The building of so many RENTAL units will no doubt decrease the value of all homes any where near that area and the traffic would be unmanageable. Also the turnover in such units is very high and would be a detriment to all homeowners around there. Please do whatever you can to stop this catastrophe from going forward.

Thank you. Dean Boyd 1315 Amelia Dr.

Drink 1 Cup Before Bed, Watch Your Body Fat Melt Like Crazy

Celebrity Local

http://thirdpartyoffers.juno.com/TGL3132/5c54d1a1abad751a168b4st04vuc

	NOTICE: This message originated outside of the City Of Cedar Falls mail system DC
NO	T CLICK on links or open attachments unless you are sure the content is safe.]

Sent: Friday, February 01, 2019 3:41 PM

To: 'Diane Lantz'

Subject: RE: Greenhill Village Apartment Project

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Hi Diane,

Thank you for your email. I just want to assure you that plans for high rise apartment buildings have not been approved. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP Planner II City of Cedar Falls, Iowa Phone: 319.268.5185

----Original Message----

From: Diane Lantz [mailto:dlantz1216@yahoo.com]

Sent: Friday, February 01, 2019 3:11 PM

To: Iris Lehmann

Subject: Greenhill Village Apartment Project

I just recently learned of plans to build eight apartment buildings near the UNI water tower in Greenhill Village. No notice has even been given to the homeowners of this area.

As a residence of Greenhill Village, I'm writing to adamantly oppose this project. When purchasing my home I believed it to be a neighborhood of privately owned homes and condos. Not a neighborhood of high rise apartment buildings that will without a doubt lower the value of our properties and overpopulate this area. Traffic has already increased significantly due to the number of apartment buildings at the corner of Greenhill and Ashworth. This will only add to what already is a disappointing and sometimes dangerous situation.

The City of Cedar Falls needs to listen to the people of this neighborhood who have invested their money and made their home in a family-oriented neighborhood not suspecting for a minute that it could become a college student community.

Sincerely,

Diane Lantz Greenhill Village

Sent from my iPhone



DEPARTMENT OF COMMUNITY DEVELOPMENT

City of Cedar Falls 220 Clay Street Cedar Falls, Iowa 50613 Phone: 319-273-8600 Fax: 319-273-8610

www.cedarfalls.com

MEMORANDUM

Planning & Community Services Division

TO: Planning and Zoning Commission

FROM: Iris Lehmann, Planner II

DATE: February 7, 2019

SUBJECT: Greenhill Village Townhomes II – Site Plan

REQUEST: Request to approve the Greenhill Village Townhomes II Site Plan

PETITIONER: Panther Farms LLC – owner; CGA Engineers – Civil Engineer

LOCATION: Lot 1 in Greenhill Village Townhomes II, 0.98 acres southeast of the Lloyd

Lane and Norse Drive intersection

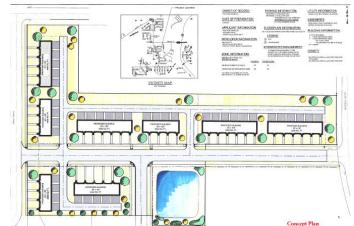
PROPOSAL

The applicant is proposing to build one (1) four-unit townhome building and one (1) five-unit townhome building on Lot 1 of the Greenhill Village Townhome II subdivision. This request will be subject to the approval of the Greenhill Village Townhomes II Preliminary and Final Plat.

BACKGROUND

In 1998 the 130 acres of the Greenhill Village property was rezoned to MU, Mixed Use Residential. This rezoning was accompanied by the creation of a Greenhill Village Master Plan that was to be used as a guide for the development in this area. Since that time there have been

a number of amendments to the Greenhill Village Master Plan. The most recent amendment to the Master Plan was approved in April 2018. This amendment to the Master Plan focused on the 5.3 acres southeast of the Greenhill Road and Hudson Road intersection; the land now described as Greenhill Village Townhomes II. The approved amendment allows for multi-unit, residential development at a density of 7.7 units an acre. The concept plan that was approved with this update to the Greenhill Village Master Plan presented a development of townhomes, see image to the right.



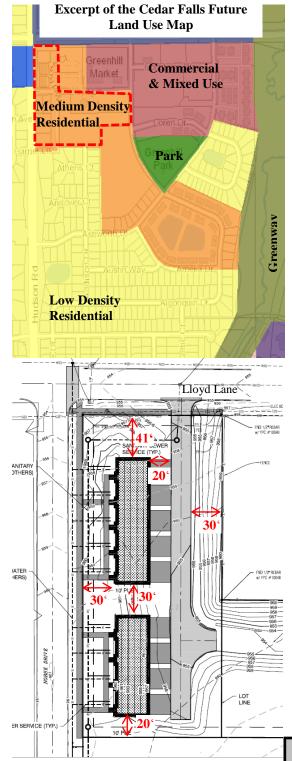
ANALYSIS

The property in question is located within the MU, Mixed Use Residential, zoning district. Development in an MU zoning district requires a detailed site plan review to ensure that the development site satisfies the standards of the comprehensive plan, recognizes principles of civic design, land use planning, landscape architecture, and building architectural design that are set out for the district. Attention to details such as parking, open green space, landscaping, signage, building design, and other similar factors help to ensure orderly development. The following is a review of the zoning ordinance requirements:

Use: The intent of the MU district is to encourage a variety of housing types and neighborhood commercial land uses for the purpose of creating viable, self-supporting neighborhood districts. Therefore, MU zoning permits a variety of uses ranging from neighborhood commercial to office to single-unit homes to condominiums. The approved Greenhill Village Master Plan, attached, arranges the various permitted uses and densities by area. The proposed Greenhill Village Townhomes II Site Plan proposes to build two townhome buildings that are consistent with the approved Greenhill Village Master Plan and concept plan. City's Future Land Use Map closely follows the Greenhill Village Master Plan. A standard planning practice is to create a gradual transition of development intensity from single unit development to higher density residential to neighborhood-serving commercial and mixed-uses. The City's Future Land Use Map shows this area. outlined in red in the image to the right, as being developed as Medium Density Residential. The proposed Greenhill Village II Site Plan will serve as a transition in development intensity and is consistent with the City's Future Land Use Map. The proposed use of Townhomes is a permitted use in this area.

Building Location: In the MU Zoning District a 30 foot minimum setback area consisting of open landscaped green space must be established around the district. In addition, principal structures must be setback at least 20 feet from any interior streets and other buildings. The boundaries of the Greenhill Village MU zoning district run along Lloyd Lane and Norse Drive. The proposed setbacks are enhanced over an excerpt of the submitted site plan in the image to the right. The locations of the two buildings meet or exceed the standards of the district.

<u>Parking:</u> The parking requirement for townhomes in Cedar Falls is two parking spaces per dwelling unit,



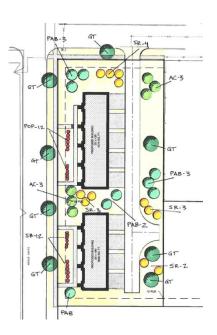
plus one additional parking space for each bedroom in each dwelling unit in excess of two bedrooms. The applicant is proposing to build one (1) four-unit townhome building and one (1) five-unit townhome building. Each unit will have three bedrooms, see floor plan below. Per the city code the development will need to provide each townhome unit a minimum of three parking spaces. In addition to this general requirement, one stall is required for every five units in excess of five units for visitor parking. There are nine units being proposed in this site plan; no guest parking is required. The proposed development will provide each unit with a tuck-under two stall garage located in the rear of the buildings and accessed from a private alley. Each garage will have a 20 foot long by 20 foot wide paved driveway. The proposed layout provides four parking spaces per unit: two in each garage and two behind each garage. These four stalls would also provide the needed parking for the optional fourth bedroom shown in the lower level next to the garage.

At the last Commission meeting, there was a concern expressed about parking congestion along neighborhood streets. The advantage of placing all the parking to the rear of the townhomes along a private alley is that it reserves the entire street frontage for visitor parking, without the interruption of multiple driveways onto the street. This site layout also helps to reduce traffic congestion as there are fewer conflict points since drivers do not have to back directly into the street. The streets are designed to accommodate parking on both sides. The location of the garages off of the back of the buildings also creates a more pedestrian-oriented, residential character along the streets which is consistent with the intent of the district. Parking lot regulations and landscaping requirements are not applicable to this review. **The parking requirement is met.**



Open Green Space/Landscaping: The MU District requires that open green space be provided at the rate of 10% of the total development site area excluding the required district setbacks. It should be noted that with no driveways located along the street frontage there is more room for landscaping and street trees in front of these homes, even though this greenspace is not counted toward the requirement. The development site is 0.98 acres or 42,689 square feet. The proposed site plan offers 0.46 acres or 20,038 square feet (47%) of open space. The minimum required open space area for this lot is 4,269 square feet. When deducting the district setbacks for this property (13,534 square feet) the open space provided for the site is 6,504 square feet. The open green space requirement is met. In addition to the greenspace requirement the MU district has a landscaping requirement of 0.02 landscaping points per square foot of total

development site area. For a 42,689 square foot lot, 854 landscaping points are needed. The proposed landscaping plan, see image to the right, is proposing to provide 2,160 landscaping points. The proposed landscaped areas will be distributed throughout the development site. The MU District also requires 0.75 landscaping points for street trees per linear foot of public street frontage. This development is required to provide 338 (415 feet x 0.75) landscaping points worth of street trees. The applicant is proposing to provide five 2 inch overstory street trees which equates to 400 landscaping points. Additional landscaping will be required as future development occurs throughout the Greenhill Village Subdivision. Staff recommends that two of the overstory "site trees" shown on the landscaping plan be relocated to the east side of Tract B, detention basin, to meet the street tree requirements for that tract. The landscaping requirements are met.



<u>Building Height</u>: The maximum building height allowed in this district is 35 feet or three stories, whichever is less. Building height is measured from the lowest point of the grade, which in the case of this design with the tuck-under garages, is at the rear. The height as measured from this lowest point is 34 feet, 11-1/4 inches. From the rear, the garage level is exposed along with the two floors of living space. However, from the street, the garage level is largely below grade, so the homes appear to be two stories in height, which is consistent with many of the two story homes in the neighborhood. **The building height requirement is met**.

<u>Building Design</u>: The MU District requires a design review of various elements to ensure architectural compatibility to surrounding structures within the MU District. Below are a set of images showing the character of neighboring buildings and developments within the MU District.



1510 Athens Ct and 1517 Athens Ct (properties to the south)



Greenhill Market and Greenhill Crossing (developments to the north)



Greenhill Village Commercial 4th (development to the east)

<u>Proportion:</u> The relationship between the width and height of the front elevations of adjacent buildings shall be considered in the construction or alteration of a building; the relationship of width to height of windows and doors of adjacent buildings shall be considered in the construction or alteration of a building.

As seen in the images on the previous page the MU District houses a variety of housing types and neighborhood commercial land uses. The property in question has single family residential development to the south and west (the land to the west is outside of the MU zoning district so an example of this housing stock is not included in this analysis). A water tower (outside of the MU zoning district), Greenhill Market (commercial), and Greenhill Crossing (multi-family development) are to the north. The Greenhill Village Commercial 4th (Multi-family development) is to the east. Two story buildings are typical for this area. Below are front and back elevations of the proposed five-unit building. Note that the proposed four-unit building has the same design, see attached for the complete set of elevations.





Each proposed townhome will be 25 feet wide by 35 feet deep. In total, the four-unit townhome building will be 100 feet long and the five-unit townhome building will be 125 feet long. The proposed design breaks up a multiunit building so each townhome is easily distinguished from the adjacent townhome with each townhome proportioned consistent with that of a single unit home. **Criterion met.**

<u>Roof shape, pitch, and direction:</u> The similarity or compatibility of the shape, pitch, and direction of roofs in the immediate area shall be considered in the construction or alteration of a building.

All neighboring residential developments have pitched roofs. The proposed two buildings incorporate the same roof shape, pitch, and direction. *Criterion met.*

<u>Pattern:</u> Alternating solids and openings (wall to windows and doors) in the front facade and sides and rear of a building create a rhythm observable to viewers. This pattern of solids and openings shall be considered in the construction or alteration of a building.

The two proposed buildings have a distinct repeating pattern for each unit on both the back and front facades. Each entry is recessed from the front line of the main portion of the townhome units to add additional depth to the front façade. Access to each unit is from a front porch. In the rear the second floor dormer overhangs the main portion of the house to also create depth in the façade. The proposed doors and windows on each side create visual interest and rhythm. *Criterion met.*

<u>Materials and texture:</u> The similarity or compatibility of existing materials and textures on the exterior walls and roofs of buildings in the immediate area shall be considered in the construction or alteration of a building. A building or alteration shall be considered compatible if the materials and texture used are appropriate in the context of other buildings in the immediate area.

To help distinguish between adjacent townhome units a vertical board and batten siding will alternate with a horizontal lap board siding from unit to unit. This variation in design also helps to prevent monotony by visually breaking up the façade into distinct units. The siding is cement board, which is a more durable material than vinyl siding. A brick liner will be used in areas where the foundation is visible. The applicant is proposing to use a standing seam metal roof. The proposed materials are consistent with materials used within the district. *Criterion met.*

<u>Color:</u> The similarity or compatibility of existing colors of exterior walls and roofs of buildings in the area shall be considered in the construction or alteration of a building.

The proposed buildings will be covered with both vertical and horizontal, white siding, gray brick, and topped with a gray roof. The use of a neutral color is consistent with the area. To provide additional visual interest, along with the alternating siding, the applicant is showing each unit with its own unique door color. **Criterion met.**

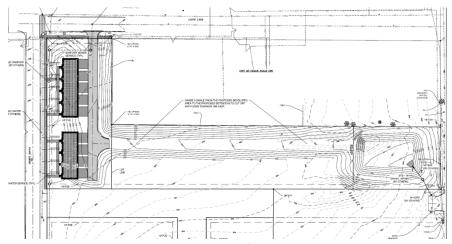
<u>Architectural features:</u> Architectural features, including but not limited to, cornices, entablatures, doors, windows, shutters, and fanlights, prevailing in the immediate area, shall be considered in the construction or alteration of a building. It is not intended that the details of existing buildings be duplicated precisely, but those features should be

regarded as suggestive of the extent, nature, and scale of details that would be appropriate on new buildings or alterations.

Each townhome unit of this building has a small porch and balcony. The roof line is broken up and the front walls are setback in places to create both visual horizontal and vertical breaks. The architectural elements of these two buildings can be found in the residential development to the south. **Criterion met.**

<u>Signage</u>: No signage is currently proposed. **The signage criterion is not applicable to this proposal.**

Access locations: A private alley is proposed coming south off of Lloyd Lane to provide access to the garages for the townhomes on Lot 1. A turnaround will be provided at a location where the private alley could continue east to serve potential future development. Public sidewalks are required along the north and west sides of Lot 1. Sidewalk connections are required along the public streets. Since there



will be a new street (Loren Drive) constructed along the south side of the subject lot in the future, a public sidewalk will also be required along the south lot line, but this can be installed when this street is constructed in the future. This future public sidewalk should be noted on the site plan prior to approval. Private sidewalk connections to the townhomes will also be provided as shown. Future public sidewalk connection should be indicated on the site plan.

Traffic Generation: At the last Commission meeting, concern was expressed about the amount of traffic that might be generated by this development. The City Engineer's Office notes that depending on the circumstances and corridor constraints, an urban two-lane roadway can handle capacities up to 1,000 vehicles per hour during peak times. The proposed subdivision shows a detention basin in the place of one of the six unit townhome buildings shown in the concept plan. With this change, the proposed development at full build-out could have 34 townhome units. A townhomes generate traffic similar to a single family home at approximately 7 trips per day per unit for a total of approximately 238 trips per day. With the new street connections proposed with this subdivision, multiple travel routes (Norse Drive, Lloyd Lane, Addison Drive, and the proposed Loren Drive) will be provided to nearby arterial and collector streets. Therefore, traffic generated by this development will not exceed the capacity of the existing streets. City staff will continue to monitor traffic volumes throughout the neighborhood and will make improvements as necessary.

Stormwater: A detention basin located east of the proposed townhomes on Outlot B of the Greenhill Village Townhomes II Subdivision will be utilized to control water runoff from this site. Outlot B will be graded to include a drainage swale that will guide the stormwater from Lot 1 to the detention basin. The detention basin is sized to hold the water run-off from a 100-year storm event and release it at a controlled rate into the City's stormsewer system. This improvement

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ensures that the water runoff from the new development will be managed. This detention basin will be sized so it has the capacity to serve any development proposed on Outlot B in the future. The City Engineer's office has reviewed the preliminary stormwater plans, but will review a more detailed report that is required prior to approval of construction drawings. The applicant's engineer will be available at the meeting to answer any technical questions about the stormwater management plan for the site. **Criterion met.**

Other Site Elements: The developer intends to remove garbage and snow from the site with a private contractor.

TECHNICAL COMMENTS

City technical staff, including Cedar Falls Utilities (CFU) personnel, has reviewed the proposed plat. Technical comments are outlined below. The applicant resubmitted documents on Tuesday February 5, 2019. These updated documents are attached to this report. By the Planning & Zoning Commission meeting on February 13, staff will confirm if the technical items have been addressed.

- 1. Enlarge all drawing plan views such that on an 11x17 sheet, they will scale properly.
- 2. Increase width of PUE to 20' around N/S sanitary main and include northern sanitary manholes/main within a PUE.
- 3. Add sidewalk along the eastern lot line of Tract B.
- 4. Add note for future sidewalk along the south side of Lot 1
- 5. On sheet C.300 remove "(By OTHERS)"
- 6. Revise and update sheet C.100 accordingly with any Preliminary Plat revisions
- 7. Submit final stormwater management report at time of construction plans.
- 8. On Sheet No.C.200:
 - a. Update the height details of the building so to be consistent with the elevations (should be 34' 11 1/4 ").
 - b. Site triangle details have not been provided.
 - c. Update required parking information. Three stalls are required per unit.
- 9. Landscaping Plan
 - Update required landscaping points so that they are just representative of Lot 1.
 Square footage used appears to be for the entire site.
 - b. Staff recommends that two of the overstory "site trees" be relocated to the east side of Tract B, detention basin to meet the street tree requirements for that tract.

A courtesy notice to adjoining property owners for this site plan was mailed on January 16, 2019. A second courtesy notice was sent to a larger area on February 4, 2019.

PLANNING & ZONING COMMISSION

Discussion 1/23/2019

Chair Oberle introduced the item and Ms. Lehmann provided background information. She explained that this site plan will be contingent upon the previously discussed preliminary plat and the submission and approval of a final plat. The lot is at the southeast corner of Lloyd Lane and Norse Drive intersection. Two townhomes are being proposed on the site.. The proposal closely follows the approved concept plan. She discussed the site plan elements, including height, setbacks, parking, access, sidewalks, landscaping, trash, stormwater management, and signage. She displayed renderings of the proposed building layout and façade elements. Staff would like to bring this to the commission for discussion only at this time.

Mr. Wingert recused himself due to a conflict of interest.

Mr. Leeper asked if there is any percentage of masonry requirements. Ms. Lehmann noted that there is not. He stated that it seems to be consistent with the rules we have set in place. Mr. Hartley noted he would like to see more about the water retention and runoff at the next meeting. Ms. Oberle noted that she feels the porches make the front more interesting. Mr. Leeper asked about the parking calculations. Ms. Lehmann stated that they are meeting and exceeding the parking requirement. Mr. Leeper then asked about street parking and whether additional onsite parking could be considered in the alley. Ms. Lehmann will discuss the option with staff and the developer.

Discussion of this item will be continued at the next meeting.

Discussion/Vote 2/13/2019

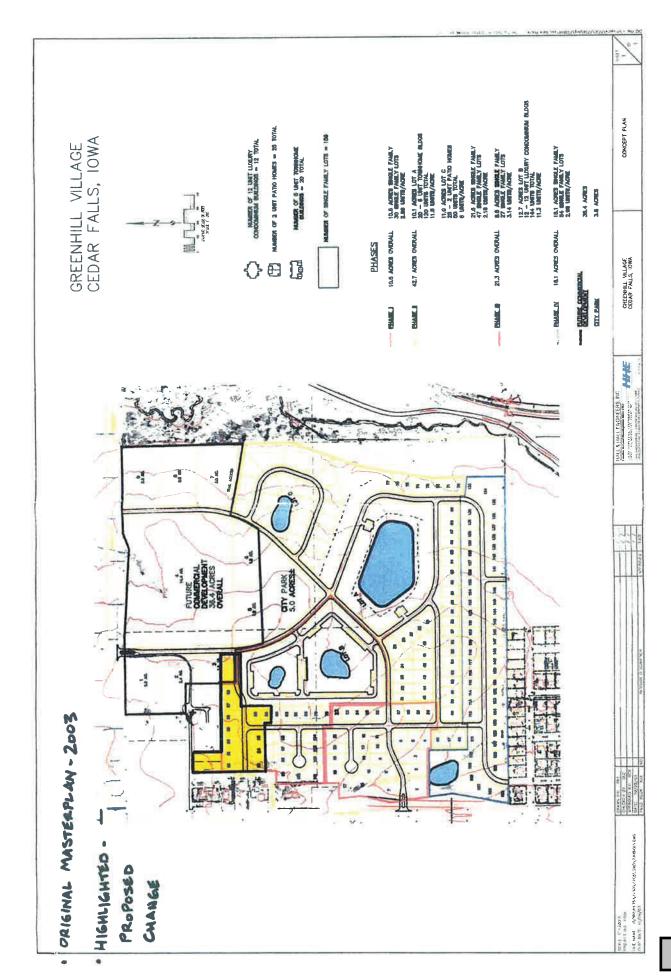
STAFF RECOMMENDATION

The proposed Greenhill Village Townhomes II Site Plan is consistent with the development intent of this area, the proposed preliminary plat, and the requirements of the zoning ordinance. Therefore, staff recommends approval, subject to the resolution of the technical comments listed above and the approval of the Greenhill Village Townhomes II Preliminary and Final Plats.

Attachments: Greenhill Village Master Plan Proposed Site Plan (Updated)

Building Elevations

Proposed Landscaping Plan Written correspondence



GREENHILL VILLAGE TOWNHOMES II LOT 1 SITE PLAN SUBMITTAL CEDAR FALLS, IOWA

2019

STARVIEWER Z
SPRINGEROOD DR
BROOKSIDER H
BRO

PROJECT LOCATION

GRADING, PAVING, & UTILITIES

WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE IOWA STATEWIDE URBAN STANDARD SPECIFICATIONS FOR PUBLIC IMPROVEMENTS, 2018 EDITION (SUDAS) AND THE CITY OF CEDAR FALLS STANDARD SPECIFICATIONS

PRELIMINARY PRELIMINARY CONSTRUCTION CONSTRUCTION

INDEX OF SHEETS

DESCRIPTION

C.001 TITLE SHEET
C.002 SYMBOLS, LEGEND AND GENERAL INFORMATION
C.100 EXISTING CONDITIONS AND DEMOLITION PLAN

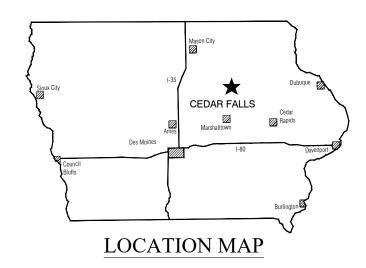
C.200 SITE PLAN

SHEET NO.

C.300 OVERALL UTILITIES AND GRADING PLAN
C.301-C.302 GRADING PLAN

C.303 SWPPP
C.500-C.504 DETAILS

VICINITY MAP NOT TO SCALE





Clapsaddle-Garber Associates, Inc 16 East Main Street, P.O. Box 754 Marshalltown, Iowa 50158 Phone 641-752-6701 www.cgaconsultants.com

SITE PLAN SUBMITTAL SET - DATE: 01-23-19



I hereby certify that this engineering document was prepared by me or under my direct personal supervisior and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.

Signature Adam C. Daters, PE

Adam C. Daters, PE lowa License Number 19579

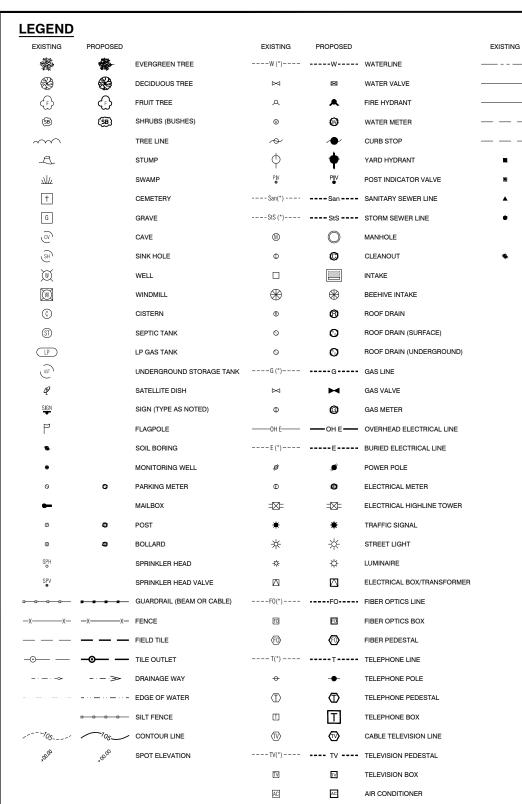
My license renewal date is December 31, 2018

· _____

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CGA PROJECT NO. 5638

SHEET C.001



UTILITY COMPANIES

ELECTRIC/GAS/WATER/TV CEDAR FALLS UTILITIES JOHN OSTERHAUS (ELECTRIC) (319) 273-8663 (319) 268-5298

JERALD LUKENSMEYER (GAS & WATER) (319) 268-5330

DAVE SCHILLING (COMMUNICATIONS)

TELEPHONE CENTURY LINK JEREMY AHRENS (319) 291-9441 DAVID SCHILLING (319) 268-5265

BRIAN HEATH (319) 268-5575

STREETS - CITY CITY OF CEDAR FALLS

CITY ENGINEER CITY OF CEDAR FALLS

(319) 268-5161

PUBLIC WORKS

CABLE BUILDING DEPARTMENT CITY OF CEDAR FALLS MEDIACOM KEVIN PARKER (319) 268-5180

STORM/SANITARY SEWER WATER RECLAMATION MIKE NYMAN (319) 268-5560

NOTE: TYPICAL DEPTH OF COVER ON WATER MAIN PIPE IN CEDAR FALLS IS 5.5 FT.

UNDERGROUND STRUCTURES, FACILITIES, AND UTILITIES HAVE BEEN PLATTED FROM AVAILABLE SURVEYS, RECORDS AND DEEDS. THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN.

ENGINEER/ LAND SURVEYOR

CLAPSADDLE-GARBER ASSOCIATES 16 E. MAIN STREET MARSHALL TOWN IA 50158 CONTACT: ADAM DATERS, PE PHONE (641) 752-6701

WATER MAIN NOTES

- MATERIAL AND CONSTRUCTION TO BE IN ACCORDANCE WITH THE "MUNICIPAL WATER UTILITY OF THE CITY OF CEDAR FALLS, IOWA" ON FILE WITH THE DEPARTMENT OF NATURAL RESOURCES. THE "CEDAR FALLS MUNICIPAL WATER DISTRIBUTION STANDARDS" AND THE "TEN STATE STANDARDS." THE CONTRACTOR SHALL VERIFY THESE REQUIREMENTS PRIOR TO BIDDING ON THIS PROJECT. IF THE STANDARDS CONFLICT, THE CONTRACTOR SHALL ABIDE BY THE MORE STRINGENT STANDARD.
- ALL WATER MAIN SHALL HAVE A MINIMUM OF 5.5 FEET OF COVER AT FINISHED GRADE IN PARKING AREAS AND 6.0 FEET IN STREET AREAS
- VALVES SHALL BE LEFT HAND OPEN RESILIENT SEAT GATE VALVES. THE COST OF VALVE BOXES ARE INCLUDED IN APPLICABLE ITEMS APPROXIMATE DEPTH OF VALVE BOXES WILL BE 7 FEET.
- 4. LEAK PRESSURE TESTS WILL BE CONDUCTED IN CONFORMANCE WITH AWWA C600-77 (CURRENT REVISION), AND SUPERVISED BY CEDAR FALLS UTILITIES
- DISINFECTION WILL BE CONDUCTED IN CONFORMANCE WITH THE IOWA STANDARDS FOR WATER SUPPLY DISTRIBUTION SYSTEMS AND THE IOWA DEPARTMENT OF NATURAL RESOURCES STANDARD SPECIFICATIONS AND SUPERVISED BY CEDAR FALLS UTILITIES. WATER MAIN SHALL NOT BE DEFLECTED MORE THAN 5° VERTICALLY OR HORIZONTALLY
- ALL WATERMAIN SHALL BE WRAPPED WITH BLUE HIGH DENSITY CROSS LINKED, 8 MIL, POLYETHYLENE WRAP IN ACCORDANCE WITH
- AWWA C105. 8. SEE CFU DETAIL FOR END OF WATER MAIN HYDRANT ASSEMBLY

WATER SYSTEM SHUTDOWN NOTIFICATION

CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WATER MAIN SHUT DOWNS WITH ANY AFFECTED CUSTOMERS AND CFU AT LEAST 48 HOURS PRIOR TO SHUTDOWN TO MINIMIZE SERVICE DISRUPTIONS. SHUT DOWNS MAY HAVE TO BE COMPLETED DURING NON-TRADITIONAL HOURS DEPENDING ON THE NEEDS OF THE AFFECTED CUSTOMERS. NO ADDITIONAL COMPENSATION WILL BE PROVIDED TO THE CONTRACTOR FOR WORK

DEMOLITION NOTES

UTILITY NOTES

PROPOSED

--- SECTION/R.O.W. LINE

BOUNDARY LINE

EASEMENT LINE

R.O.W. RAIL OR LOT CORNER

GOVERNMENT CORNER MONUMENT

PARCEL OR LOT CORNER MONUMENT

TEMP. CONSTRUCTION EASEMENT CORNER

ALL DIMENSIONS ARE TO BACK OF CURB WHERE APPLICABLE

ALL DISTURBED AREAS, UNLESS OTHERWISE INDICATED, SHALL BE SEEDED, FERTILIZED AND MULCHED WITHIN THE

CONTRACTOR TO VERIFY EXISTING UTILITY SIZES PRIOR TO CONSTRUCTION. ENGINEER SHALL BE NOTIFIED OF ANY

CONTRACTOR SHALL SUPPLY DETAILED CONSTRUCTION SCHEDULE TO PROJECT ENGINEER PRIOR TO CONSTRUCTION.
 CONTRACTOR SHALL COORDINATE RELOCATION OF EXISTING UTILITIES, WHICH ARE TO BE PERFORMED BY OTHERS,

COMMENCE CONSTRUCTION.
CONTRACTOR SHALL DISPOSE OF WASTE EARTHWORK ON-SITE, AT THE DIRECTION OF THE PROJECT ENGINEER.

11. ALL SIDEWALK AND RAMPS SHALL BE IN COMPLIANCE WITH IOWA SUDAS SIDEWALK DEISGN REQUIREMENTS AND ADA ACCESSABILITY GUIDELINES. FAILURE OF SIDEWALK TO MEET THESE REQUIRMENTS WILL REQUIRE REMOVAL AND RECONSTRUCTION AT CONTRACTOR'S COST.

PRIOR TO INSTALLING UTILITIES, CONTRACTOR SHALL EXCAVATE AND FIELD VERIFY EXACT LOCATION, SIZE, AND

ELEVATION OF ALL PUBLIC/PRIVATE UTILITIES WITHIN THE PROJECT LIMITS.
UNDERGROUND STRUCTURES, FACILITIES, AND UTILITIES HAVE BEEN PLATTED FROM AVAILABLE SURVEYS, RECORDS

EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN.

3. THE CONTRACTOR SHALL CALL IOWA ONE CALL AT (800) 292-8989 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY UTILITY LOCATIONS AND FLOW LINES PRIOR TO

AND DEEDS. THEREFORE, THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHERS, THE

ALL WORK SHALL BE IN ACCORDANCE WITH OSHA CODES AND STANDARDS. NOTHING INDICATED ON THESE DRAWINGS

APPROPRIATE SEEDING PERIOD. SEED SHALL BE TYPE 1 LAWN MIXTURE. SEE SUDAS SECTION 9010. SEEDED SLOPES SHALL BE 4:1. MAXIMUM SLOPE ALLOWED SHALL BE 3:1 WHERE APPROVED BY ENGINEER

SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH ANY APPROPRIATE SAFETY REGULATIONS ONE WEEK PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER OF THEIR INTENT TO

10. STORMWATER POLLUTION PREVENTION INFORMATION INCLUDED IN THIS PLAN SET IS NOT THE COMPLETE STORMWATER POLLUTION PREVENTION PLAN (SWPPP), BUT RATHER A PART OF THE SWPPP THAT IS TO BE UPDATED

CONCRETE MONUMENT

SURVEY CONTROL POINT

WITH RESPECTIVE LITILITY COMPANIES

GENERAL NOTES

- THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY ACCESS OR CONSTRUCTION ACTIVITIES TO ORIGINAL CONDITIONS. THIS MAY INCLUDE SCARIFYING AND RESEEDING OR OTHER RESTORATION EFFORTS AS REQUIRED. RESTORATION SHALL BE INCIDENTAL TO THE PROJECT.
- ADJOINING PROPERTY SHALL BE PROTECTED DURING DEMOLITION OPERATIONS. DEBRIS SPILLAGE ON ADJOINING PROPERTIES SHALL BE CAREFULLY REMOVED BY THE CONTRACTOR.
- 3. ALL CONSTRUCTION DEBRIS SHALL BE REMOVED FROM SITE AND PROPERLY DISPOSED OF

CONSTRUCTION AND TO PROTECT ALL UTILITIES FROM DAMAGE.

SHOP DRAWING SUBMITTALS

- PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER ALL SHOP DRAWINGS, MATERIAL CERTIFICATIONS AND VENDOR MATERIALS CERTIFICATIONS FOR ALL MATERIALS TO BE INCORPORATED INTO THE
- CONTRACTOR SHALL ALLOW 14 CALENDAR DAYS FOR REVIEW OF SHOP DRAWINGS. MATERIAL CERTIFICATIONS AND VENDOR MATERIALS CERTIFICATIONS. IF REVIEW HAS NOT BEEN COMPLETED IN 14 CALENDAR DAYS, THIS SHALL NOT IMPLY APPROVAL, CONTACT PROJECT ENGINEER FOR SUBMITTAL STATUS.
- 3. CONTRACTOR SHALL NOT BE COMPENSATED FOR ANY MATERIAL THAT IS INSTALLED PRIOR TO APPROVAL BY THE ENGINEER. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COST TO REPLACE ANY NON-APPROVED MATERIALS.



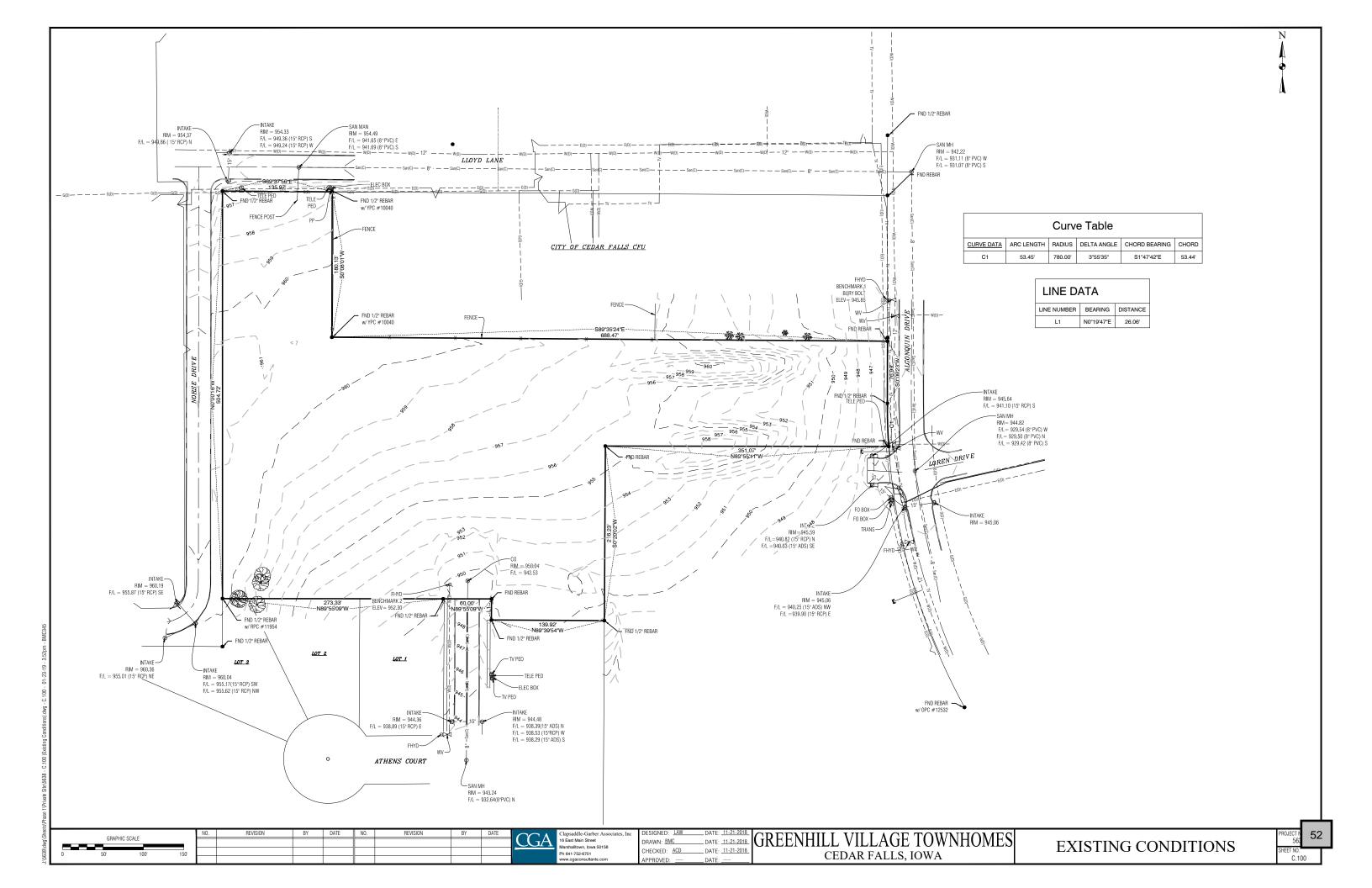


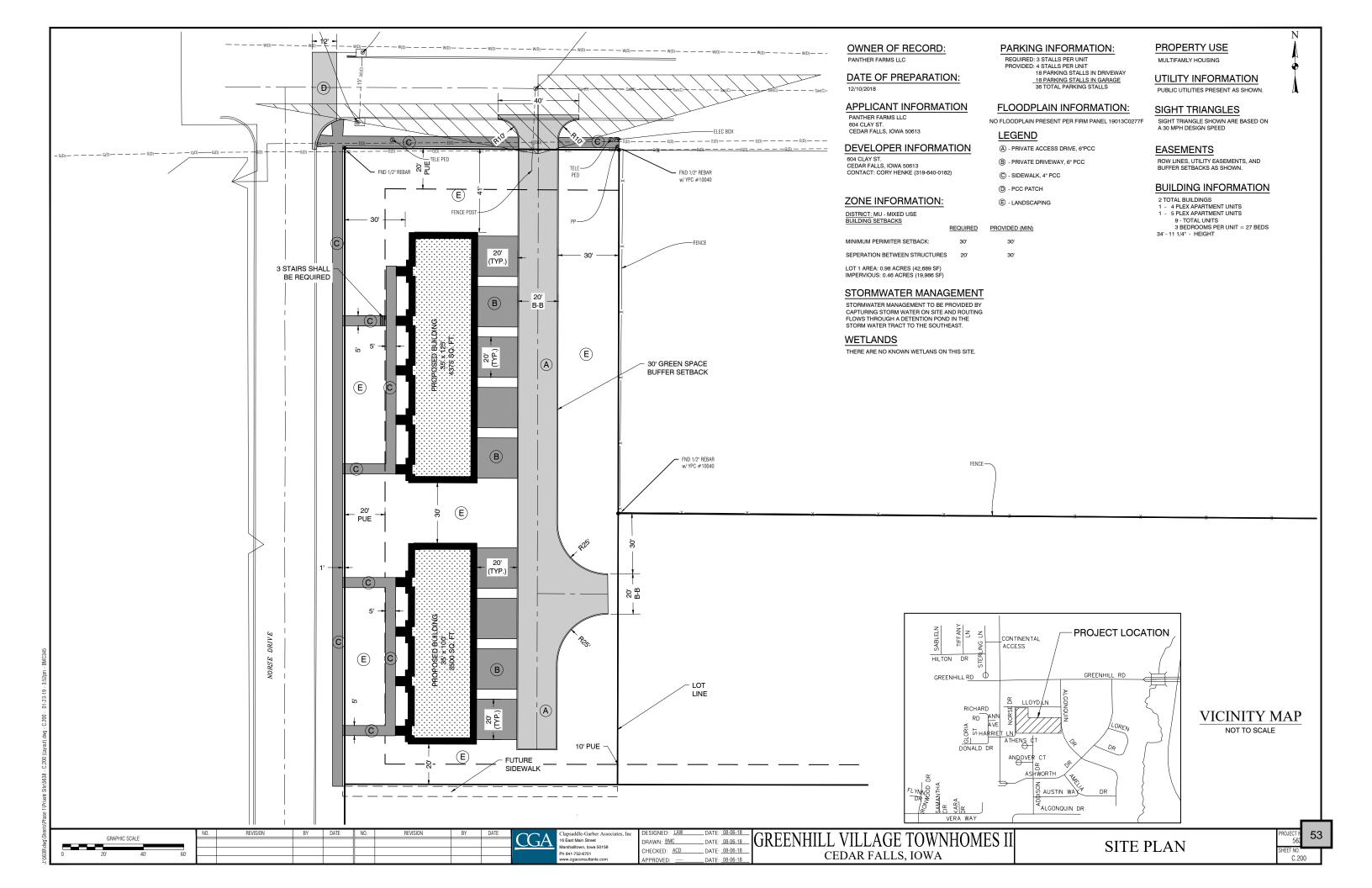
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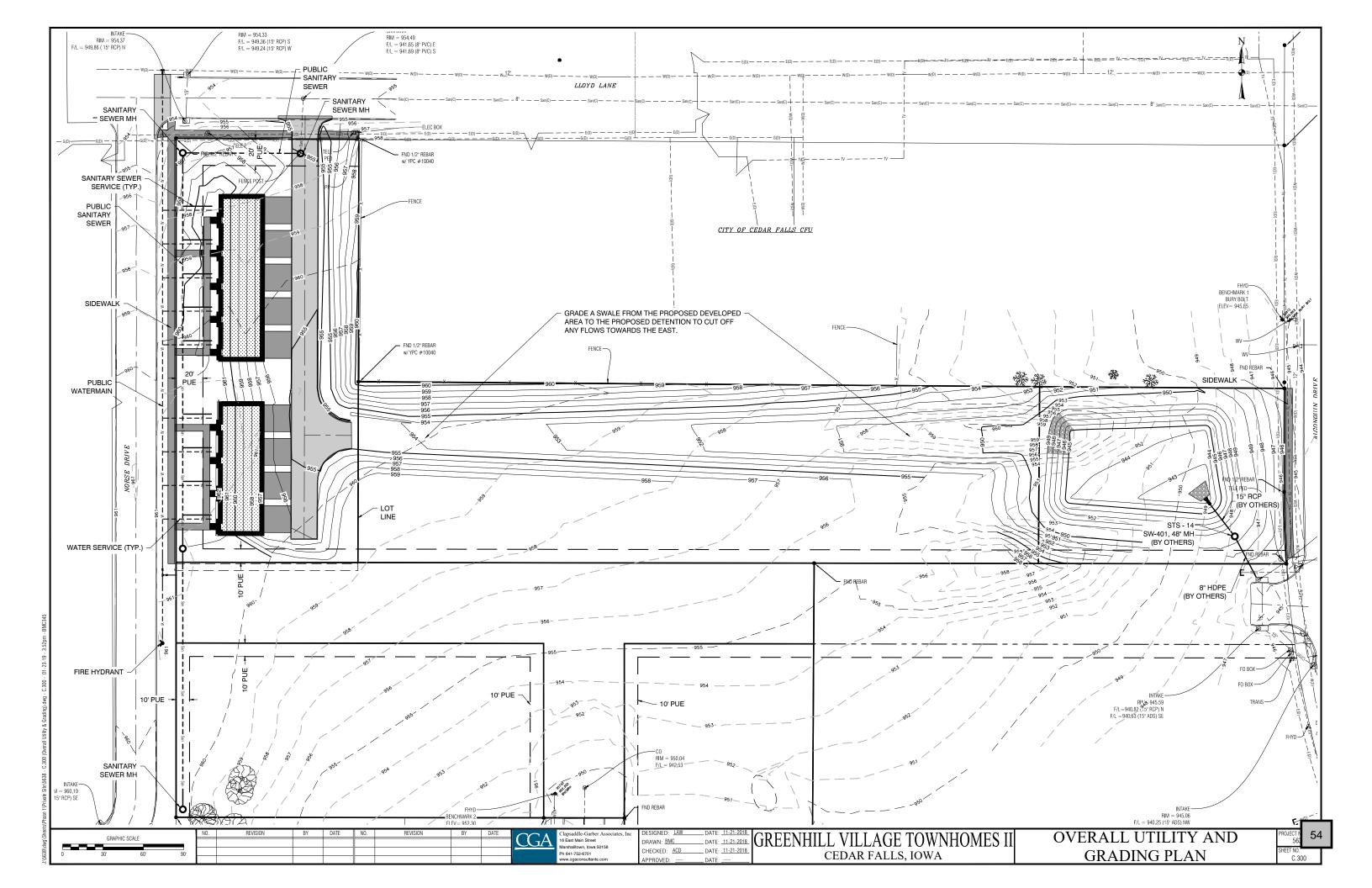
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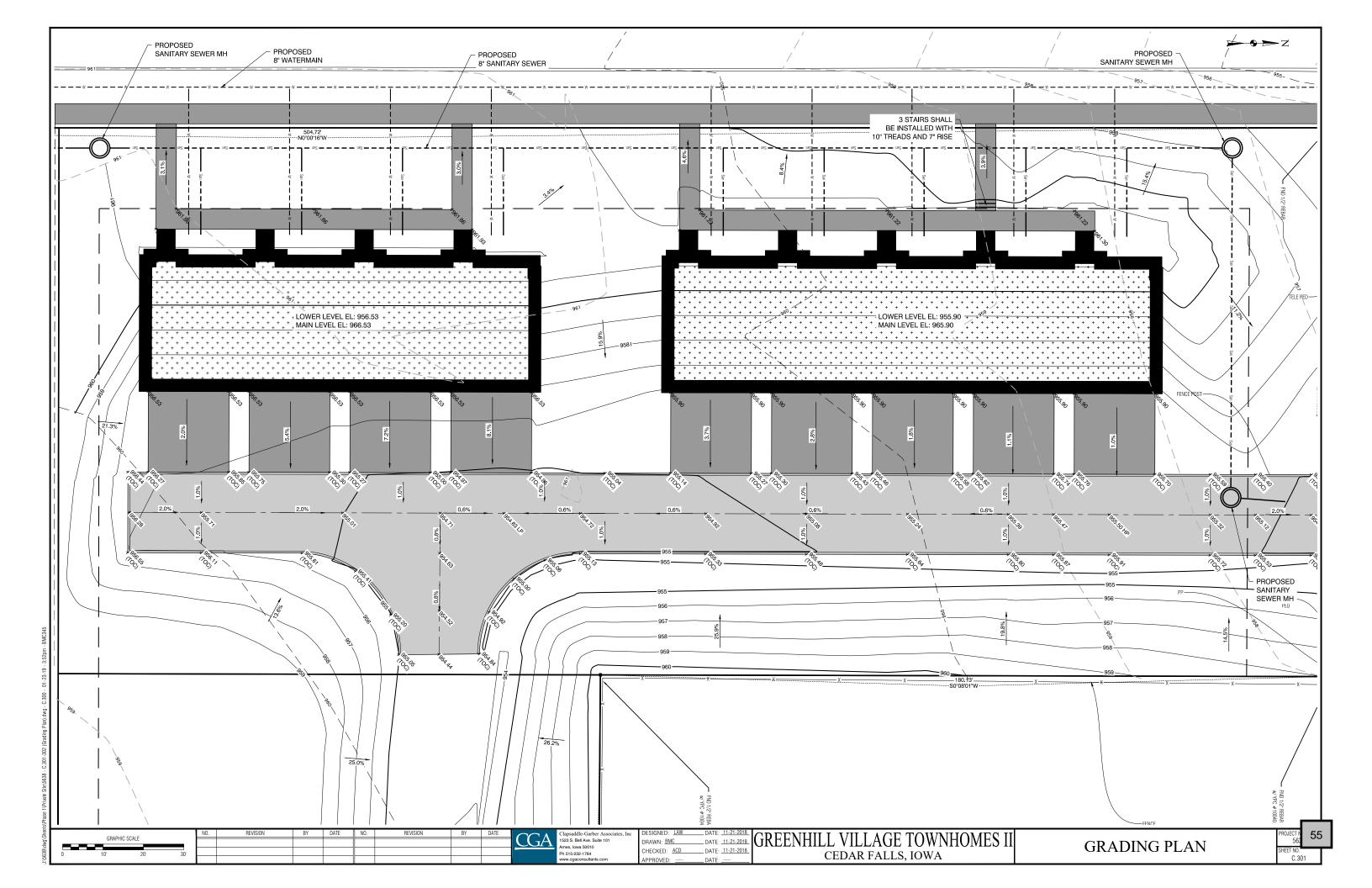
GREENHILL VILLAGE TOWNHOMES II CEDAR FALLS, IOWA

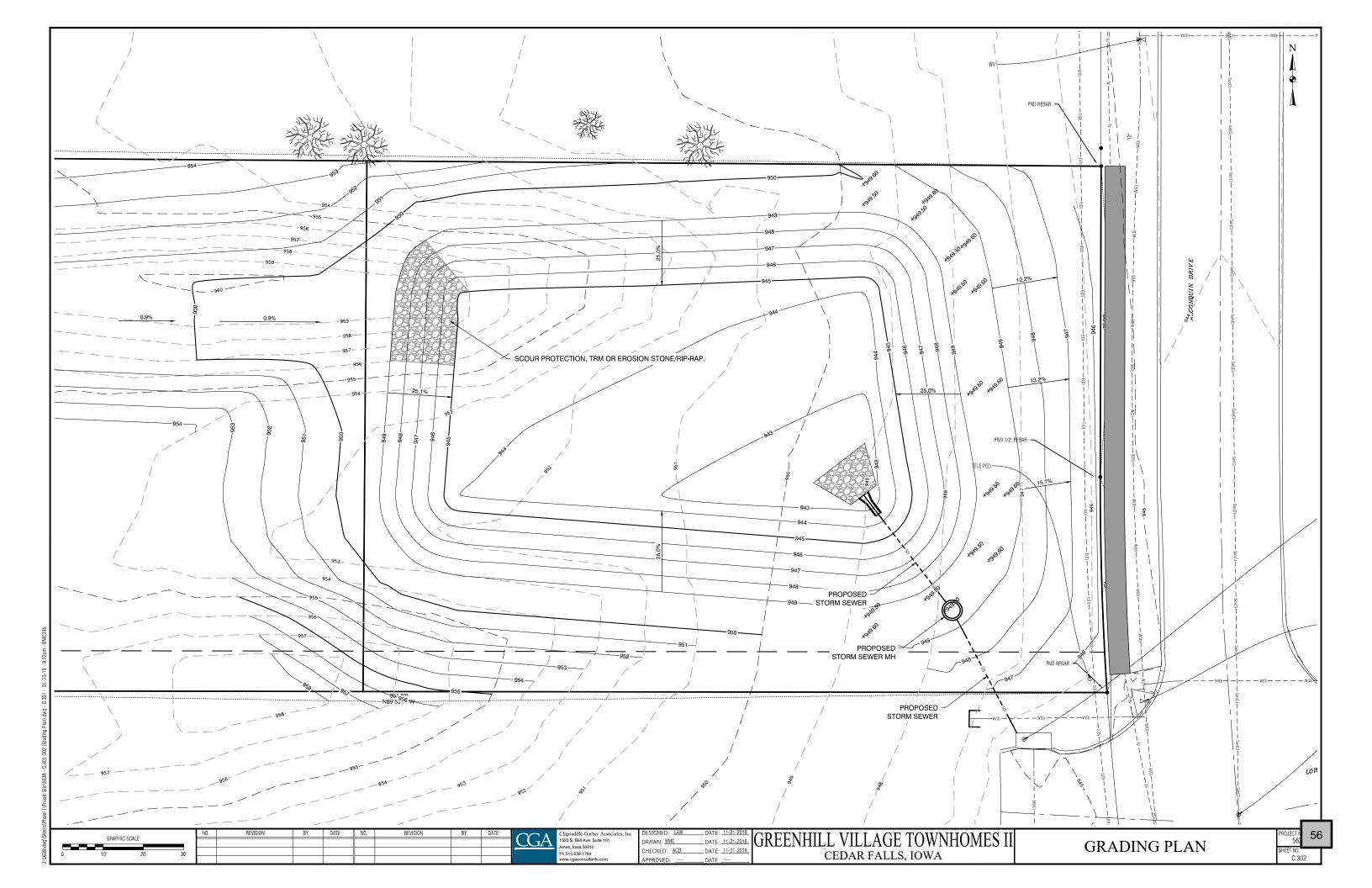
SYMBOLS, LEGEND, AND **GENERAL INFORMATION**









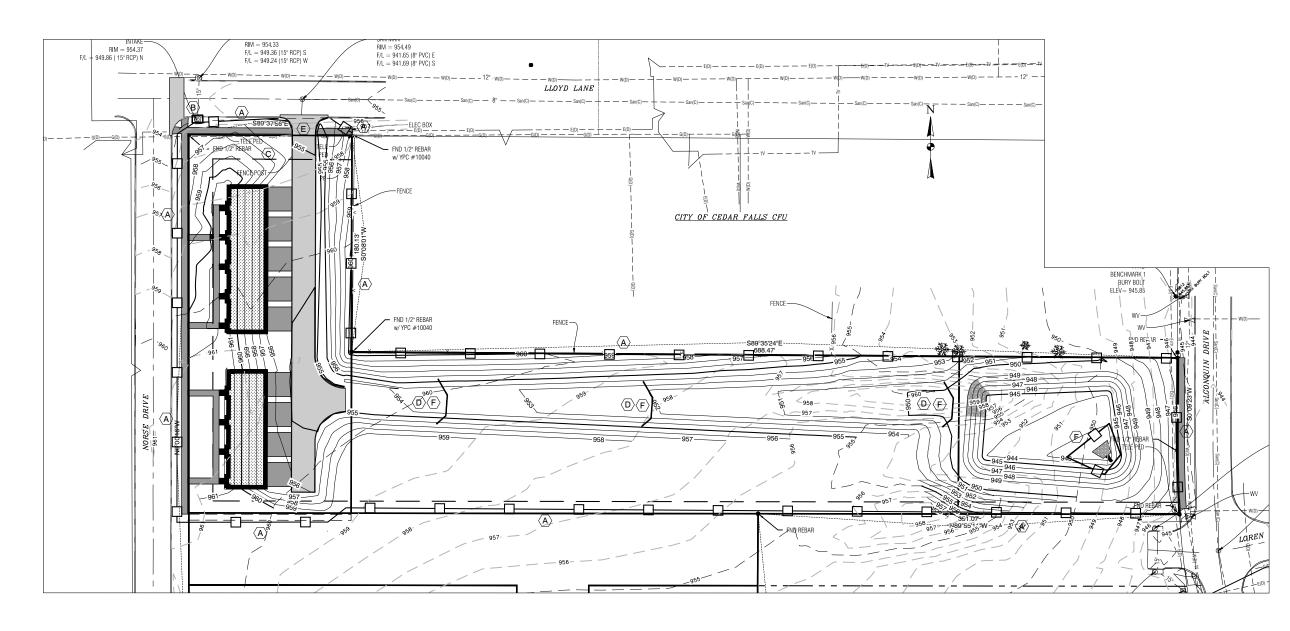


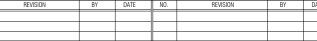
POLLUTION PREVENTION PLAN NOTES

- 1. ALL CONTRACTORS SHALL BE SUPPLIED A COPY OF THE SWPPP DOCUMENT AT THE BEGINNING OF CONSTRUCTION. A COPY OF THE SWPPP DOCUMENTS SHALL BE LOCATED WITH THE SWPPP MANAGER AT ALL TIME. COPIES CAN ALSO BE FOUND AT THE OFFICES OF 16 E MAIN STREET, MARSHALLTOWN, IA 50158. ALL CONTRACTORS/SUBCONTRACTORS SHALL CONDUCT THEIR OPERATIONS IN A MANNER THAT MINIMIZES EROSION AND PREVENTS SEDIMENTS AND NON EARTH DISTURBING POLLUTANTS FROM LEAVING THE SITE AS DESCRIBED IN THE SWPPP. THE PRIME CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE AND IMPLEMENTATION OF THE SWPPP FOR THEIR ENTIRE CONTRACT. THIS RESPONSIBILITY SHALL BE FURTHER SHARED WITH SUBCONTRACTORS WHOSE WORK IS A SOURCE OF POTENTIAL POLLUTION AS DEFINED IN THE SWPPP.
- 2. THIS SHEET IS NOT THE COMPLETE STORM WATER PREVENTION PLAN (SWPPP), BUT RATHER A PART OF THE SWPPP THAT IS TO BE UPDATED REGULARLY BY THE CONTRACTOR. IT IS THE PRIME CONTRACTORS RESPONSIBILITY TO UPDATE THE SWPPP PLAN AS NEEDED AS WELL AS CONDUCT ANY NECESSARY INSPECTIONS IN ACCORDANCE WITH THE CITY OF CEDAR FALLS, IOWA DNR AND EPA GUIDELINES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IDENTIFYING ANY DEFICIENCIES, CORRECTING THOSE DEFICIENCIES IMMEDIATELY AND DOCUMENTING SUCH WITH THE SWPPP. ALL EROSION CONTROL ITEMS SHALL BE REMOVED AT THE END OF THE PROJECT. THE SWPPP MANAGER SHALL UPDATE SITE PLAN WITH THE LOCATION OF THE "POLLUTION PREVENTION PLAN LEGEND" ITEMS ONCE THE LOCATION OF THOSE CONTROLS IS ESTABLISHED. ANY ADDITIONAL CONTROLS THAT ARE USED BUT ARE NOT LISTED IN THE LEGEND SHALL BE CLEARLY IDENTIFIED ON THE PLANS. POST CONSTRUCTION STORM WATER CONTROL (DETENTION) IS LOCATED OFFSITE.

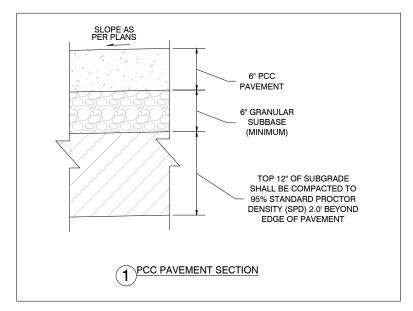
LEGEND

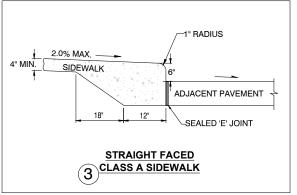
- $\overleftarrow{\left(A\right)}$ INSTALL SILT FENCE OR COMPOST FILLED FILLED SOCK TO BE REMOVED AT THE END OF THE PROJECT.
- B INSTALL INLET PROTECTION TO BE REMOVED AT THE END OF THE PROJECT.
- $\overleftarrow{\mathbb{C}}$ Install concrete wash out area to be removed & restored at the end of the project.
- $\langle \overline{D} \rangle$ INSTALL EXCELSIOR MAT
- $\left\langle \overline{E}\right\rangle$ INSTALL STABILIZED CONSTRUCTION ENTRANCE.
- F INSTALL FILTER SOCKS/DITCH CHECKS

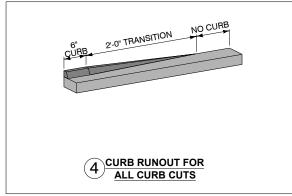












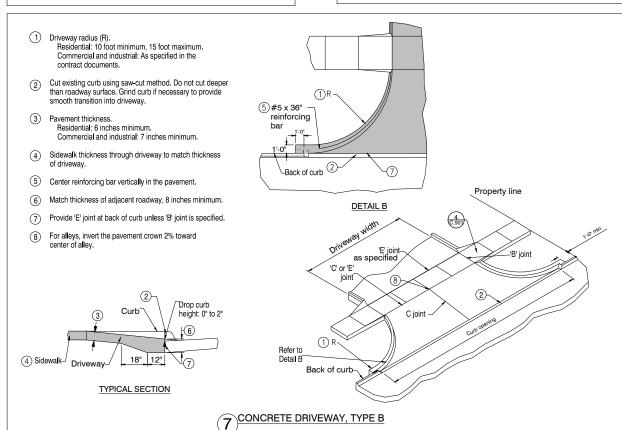
Flapsaddie-Garber Asso 6 East Main Street Marshalltown, Iowa 50158 Ph 641-752-6701

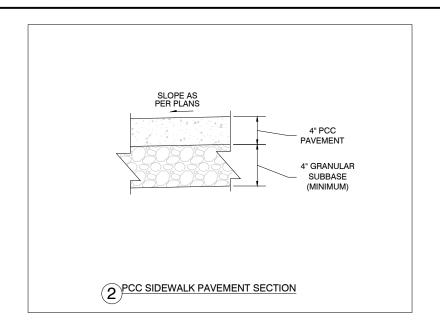
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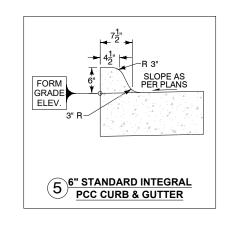
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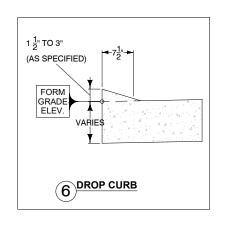
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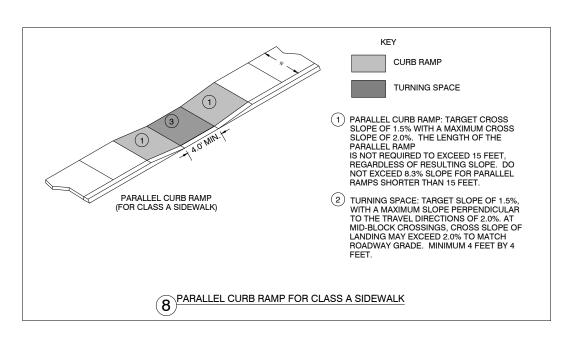
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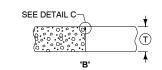




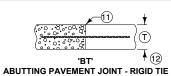




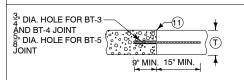




PLAIN JOINT (ABUTTING DISSIMILAR PAVEMENT SLABS)

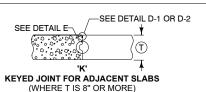


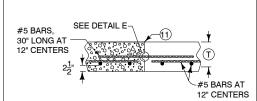
T	JOINT	BARS	BAR LENGTH AND SPACING
< 8"	'BT-1'	#4	36" LONG AT 30" CENTERS
≥ 8"	'BT-2'	#5	36" LONG AT 30" CENTERS



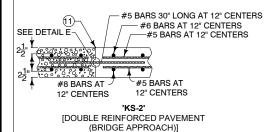
ABUTTING PAVEMENT JOINT - RIGID TIE (DRILLED)

T	JOINT	BARS	BAR LENGTH AND SPACING
< 8"	'BT-5'	#4	24" LONG AT 30" CENTERS
> 8"	'BT-3'		24" LONG AT 30" CENTERS
≥ 0	'BT-4'	#5	24" LONG AT 15" CENTERS

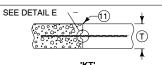




'KS-1' [SINGLE REINFORCED PAVEMENT (BRIDGE APPROACH)]

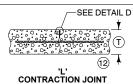


LONGITUDINAL CONTRACTION JOINTS

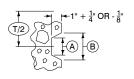


10(12) ABUTTING PAVEMENT JOINT - KEYWAY TIE

T	JOINT	BARS	BAR LENGTH AND SPACING
< 8"	'KT-1'	#4	30" LONG AT 30" CENTERS
> 8"	'KT-2'	#5	30" LONG AT 30" CENTERS
≥ 0	'KT-3'	#5	30" LONG AT 15" CENTERS

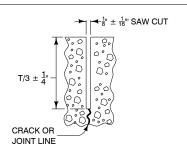


T	JOINT	BARS	BAR LENGTH AND SPACING
< 8"	'L-1'	#4	36" LONG AT 30" CENTERS
- 011	'L-2'	-4-г	36" LONG AT 30" CENTERS
≥ 8"	'L-3'	#5	36" LONG AT 15" CENTERS

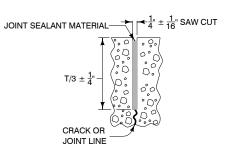


DETAIL E

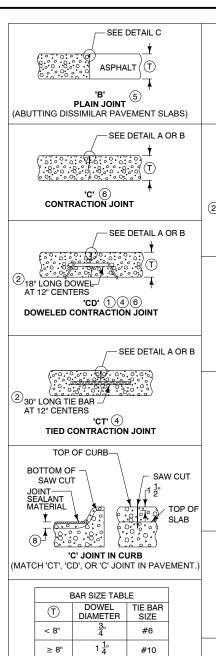
KEYWAY DIMENSIONS							
KEYWAY TYPE	PAVEMENT THICKNES\$T	A	В				
STANDARD	8" OR GREATER	1 <mark>3</mark> "	2 3 "				
NARROW	LESS THAN 8"	1"	2"				

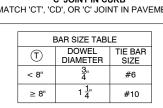


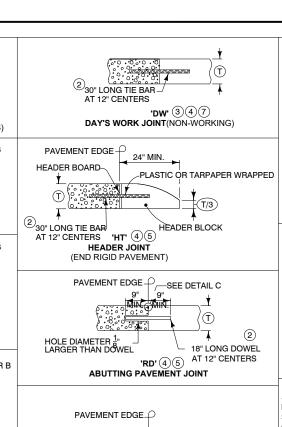
DETAIL D-1 (REQUIRED WHEN THE DEPARTMENT OF TRANSPORTATION IS THE CONTRACTING AUTHORITY, OR WHEN SPECIFIED IN THE CONTRACT DOCUMENTS.)



DETAIL D-2 (REQUIRED WHEN THE DEPARTMENT OF TRANSPORTATION IS THE CONTRACTING AUTHORITY, OR WHEN SPECIFIED IN THE CONTRACT DOCUMENTS.)



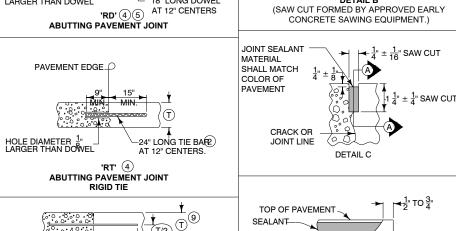




← L/2 **→**

BAR PLACEMENT

(APPLIES TO ALL JOINTS UNLESS OTHERWISE DETAILED.)



JOINT SEALANT

MATERIAL SHALL

SAW CUT

CRACK OR -

JOINT SEALANT

MATERIAL SHALL

CRACK OR-

JOINT LINE

MATCH COLOR OF PAVEMENT

DETAIL A (SAW CUT FORMED BY CONVENTIONAL

CONCRETE SAWING EQUIPMENT.)

DETAIL B

SECTION A-A

(DETAIL AT EDGE OF PAVEMENT)

MATCH COLOR

OF PAVEMENT

 $\pm \frac{1}{16}$ " SAW CUT

 $+\frac{1}{8}$ " TO $\frac{5}{16}$ " SAW CUT

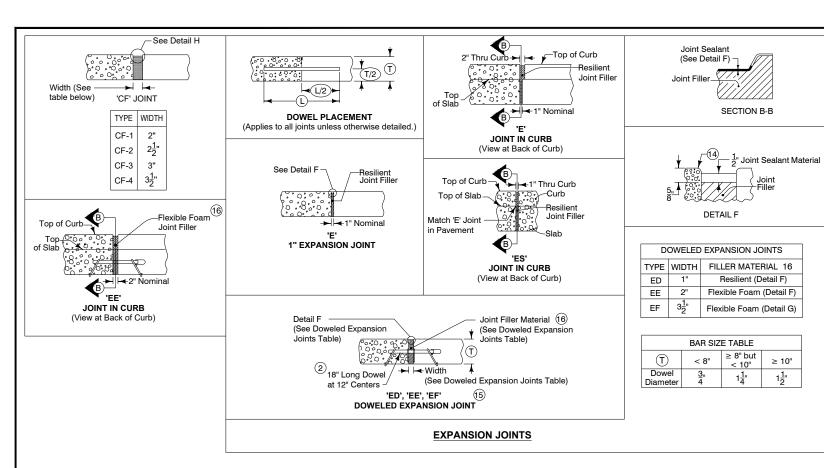
TRANSVERSE CONTRACTION JOINTS



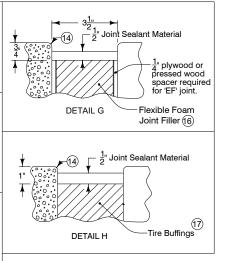
h 641-752-6701

_DATE: <u>06-29-2017</u> RAWN: BMC CHECKED: ACD DATE: 06-29-2017 DATE: 06-29-20

GREENHILL VILLAGE TOWNHOMES II CEDAR FALLS, IOWA



	Туре			Method of Load Transfer				Thermal movement					
Joint	Transverse	Longitudinal	Expansion	Aggregate Interlock	Key	Tie Bar	Dowel Bar	Doweled to allow movement	Tied to prevent movement	Expansion joint allows movement	Lack of reinforcing allows movement	Comments	
В	х	х									х	Used between dissimilar materials or when other joints are not suitable.	
С	х			х							х	Transverse joint used when T < 8 in. May also be used on non-primary routes if AADIT < 200 vpd.	
CD	х			х			х	х				Transverse joint used when T ≥ 8 in. Use C joint when joint length is 2 ft.	
СТ	х			х		х			х			Speciality tied contraction joint.	
DW	х					х			х			Used by contractor as a stopping point.	
нт	х					х			х			Used at the end of rigid pavement prior to placement of second slab.	
RD	х						х	х				Joint between new and existing pavements, dowels are used.	
RT	х					х			х			Joint between new and existing pavements, tie bars are used.	
BT-1		х							х			Longitudinal joint used when T < 8 in, interchangeable with L-1 depending on paving sequence.	
3T-2		х							х			Used when L-2 and the KT-2 are not possible, T ≥ 8 in.	
3T-3		х							х			Joint used between new and existing pavements. Tie bars are used when T≥8 in.	
BT-4		х							х			Joint used between new and existing pavements. Tie bars are used when T≥8 in.	
BT-5		х							х			Joint used between new and existing pavements. Tie bars are used when T < 8 in.	
K		х			х						X T > 8 in. minimal usage.		
KS		х			х				х			Used in reinforced pavements.	
KT-1		х			х				х			Longitudinal joint used when T < 8 in, interchangeable with L-1 depending on paving sequence.	
KT-2		х			х				х			Longitudinal joint used when T < 8 in, interchangeable with L-2 depending on paving sequence.	
KT-3		х			х				х			Longitudinal joint used when T < 8 in, interchangeable with L-3 depending on paving sequence.	
1		х		х					х			Longitudinal Joint used when T < 8 in, interchangeable with BT-1.	
L-2		х		х					х			Longitudinal joint used when T ≥ 8 in, interchangeable with KT-2 depending on paving sequence.	
L-3		х		х					х			Longitudinal joint used with pavement of large width, interchangeable with KT-3 depending on paving sequence.	
CF	х		х							х		4 in expansion joint.	
E	х	х	х							х		1 in expansion joint.	
E	х		х				х	X X 1 in doweled expansion joint.		1 in doweled expansion joint.			
E	х		х				х	X X 2 in doweled expansion joint.		2 in doweled expansion joint.			
EF	х		х				х			4 in doweled expansion joint			
S			х							х		Used in curb to match expansion joint in pavement.	



NOTES:

- PAVEMENT JOINT TYPES ARE DEPENDENT UPON POUR SEQUENCE.
- 2. JOINTS SHALL BE A MINIMUM OF 2' IN LENGTH AND HAVE AN ANGLE OF 70° OR MORE.
- 3. MAXIMUM JOINT SPACING IS AS FOLLOWS: TRANSVERSE = 15' LONGITUDINAL = 12.5'
- (1) SEE DOWEL ASSEMBLIES FOR FABRICATION DETAILS.
- 2 SEE BAR SIZE TABLE.
- (3) LOCATE 'DW' JOINT AT A MID-PANEL LOCATION BETWEEN FUTURE 'C' OR 'CD' JOINTS. PLACE NO CLOSER THAN 5 FEET TO A 'C' OR 'CD' JOINT.
- (4) PLACE BARS WITHIN THE LIMITS SHOWN UNDER DOWEL ASSEMBLIES.
- (5) EDGE WITH 1/4 INCH TOOL FOR LENGTH OF JOINT INDICATED IF FORMED; EDGING NOT REQUIRED WHEN CUT WITH DIAMOND BLADE SAW. REMOVE HEADER BLOCK AND BOARD WHEN SECOND SLAB IS PLACED.
- (6) UNLESS OTHERWISE SPECIFIED, USE 'CD' TRANSVERSE CONTRACTION JOINTS IN MAINLINE PAVEMENT WHEN (T) IS GREATER OR EQUAL TO 8 INCHES. USE 'C' JOINTS WHEN (T) IS LESS THAN 8 INCHES.
- (7) 'RT' JOINT MAY BE USED IN LIEU OF 'DW' JOINT AT THE END OF THE DAYS WORK. REMOVE ANY PAVEMENT DAMAGED DUE TO THE DRILLING AT NO ADDITIONAL COST TO THE CONTRACTING AUTHORITY.
- (8) SAW 'CD' JOINT TO A DEPTH OF T/3 \pm 1/4"; SAW 'C' JOINT TO A DEPTH OF T/4 \pm 1/4".
- $\ensuremath{\textcircled{9}}$ WHEN TYING INTO OLD PAVEMENT, $\ensuremath{\textcircled{\texttt{T}}}$ REPRESENTS THE DEPTH OF SOUND PCC.
- (10) BAR SUPPORTS MAY BE NECESSARY FOR FIXED FORM PAVING TO ENSURE THE BAR REMAINS IN A HORIZONTAL POSITION IN THE PLASTIC CONCRETE.
- (1) SAWING OR SEALING OF JOINT NOT REQUIRED.
- (12) THE FOLLOWING JOINTS ARE INTERCHANGEABLE, SUBJECT TO THE POURING SEQUENCE:

 "BT-1", "L-1", AND "KT-1"

 "KT-2" AND "L-2"

 "KT-3" AND "L-3"
- (13) SEALANT OR CLEANING NOT REQUIRED.
- (4) EDGE WITH 1/4 INCH TOOL FOR LENGTH OF JOINT INDICATED IF FORMED; EDGING NOT REQUIRED WHEN CUT WITH DIAMOND BLADE SAW.
- (5) SEE DOWEL ASSEMBLIES FOR FABRICATION DETAILS AND PLACEMENT LIMITS. COAT THE FREE END OF DOWEL BAR TO PREVENT BOND WITH PAVEMENT. AT INTAKE LOCATIONS, DOWEL BARS MAY BE CAST-IN-PLACE.
- (16) PREDRILL OR PREFORM HOLES IN JOINT MATERIAL FOR
- (17) COMPACT TIRE BUFFINGS BY SPADING WITH A SQUARE-NOSE SHOVEL.



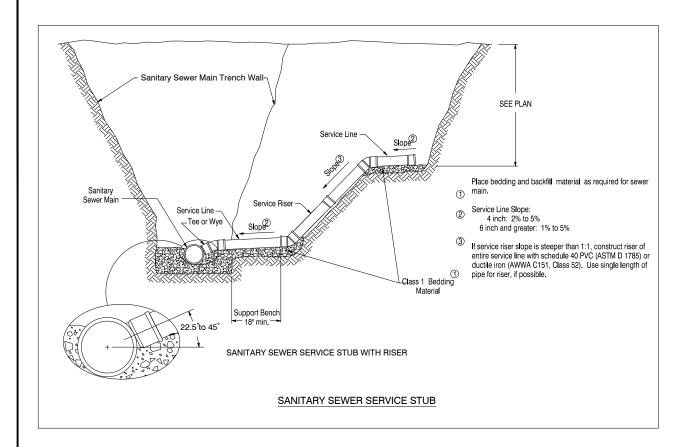
Clapsaddle-Garber Associates, Inc 16 East Main Street Marshalltown, Iowa 50158 Ph 641-752-6701
 DESIGNED:
 LAW
 DATE:
 06-29-2017

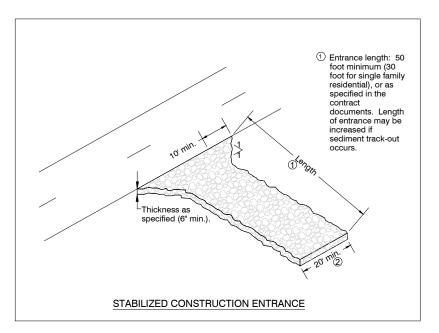
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 06-29-2017

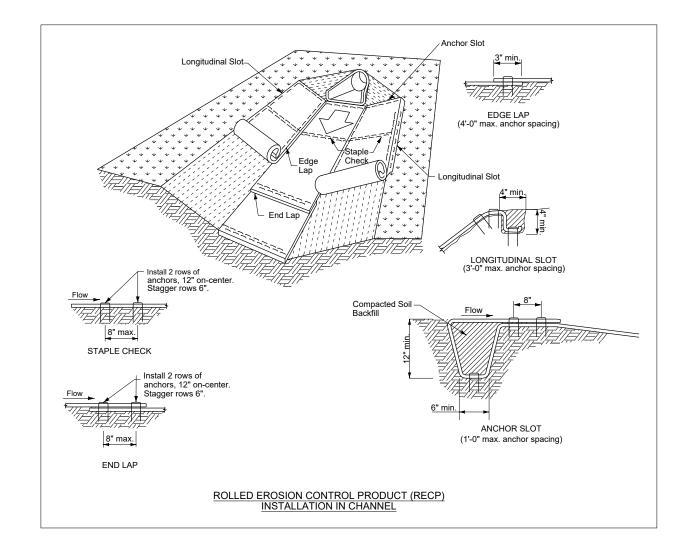
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 DATE:
 06-29-2017





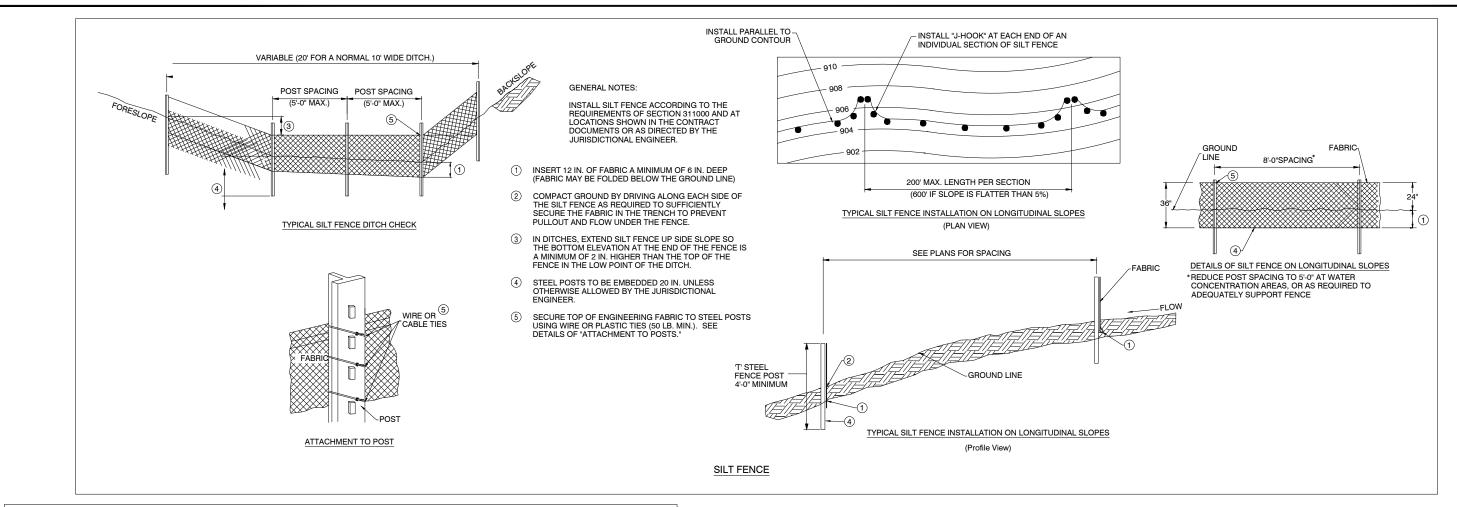






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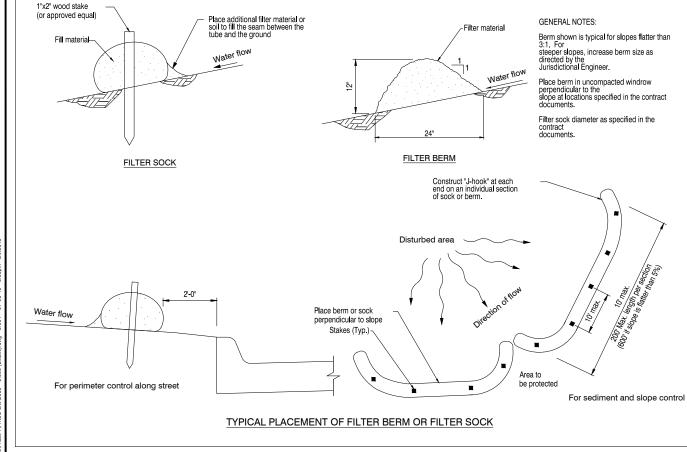
East Main Street arshalltown, Iowa 50158

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_DATE: _06-29-201



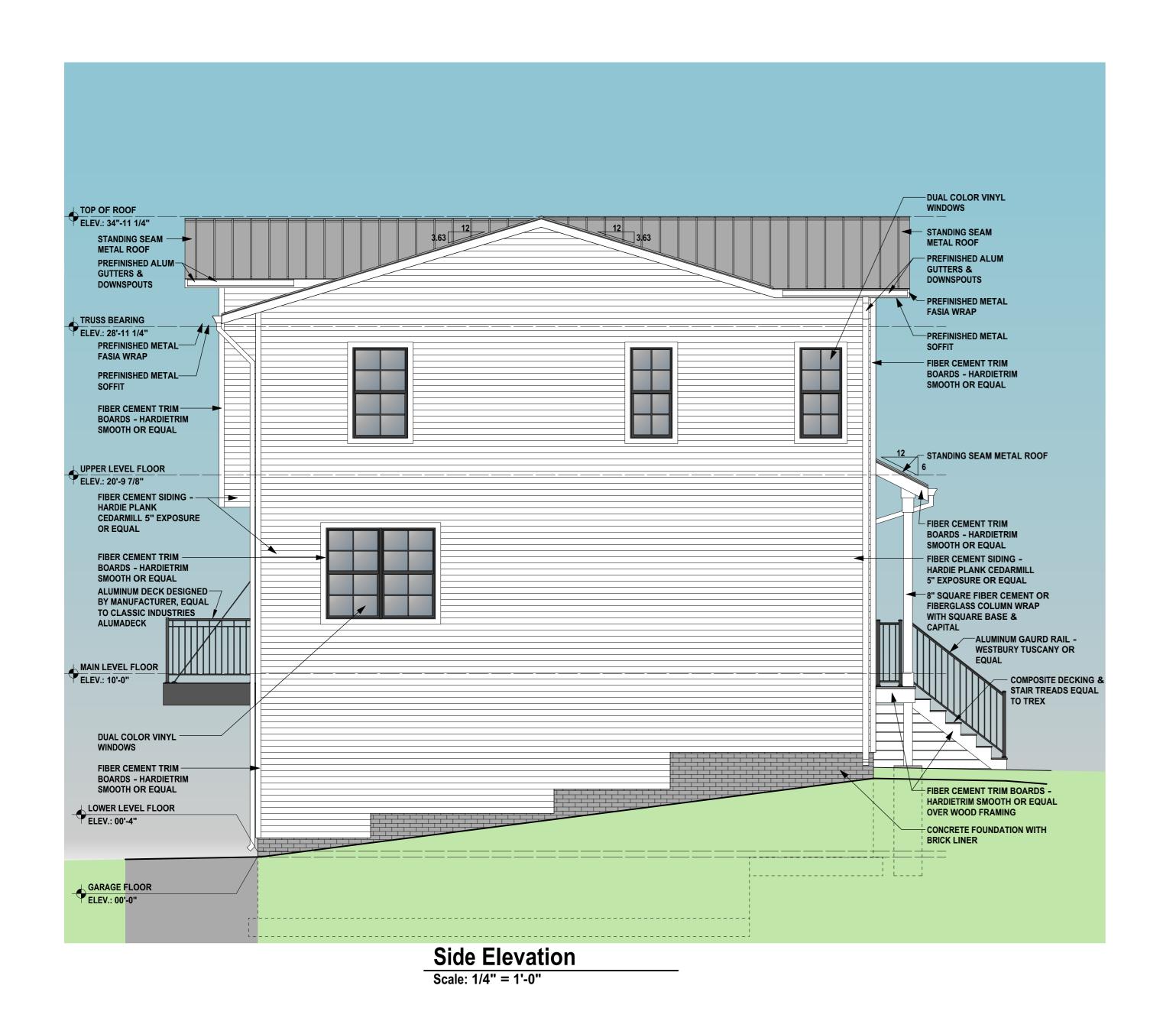
GREENHILL VILLAGE TOWNHOMES II DATE: 06-29-2017 DATE: 06-29-2017 CEDAR FALLS, IOWA

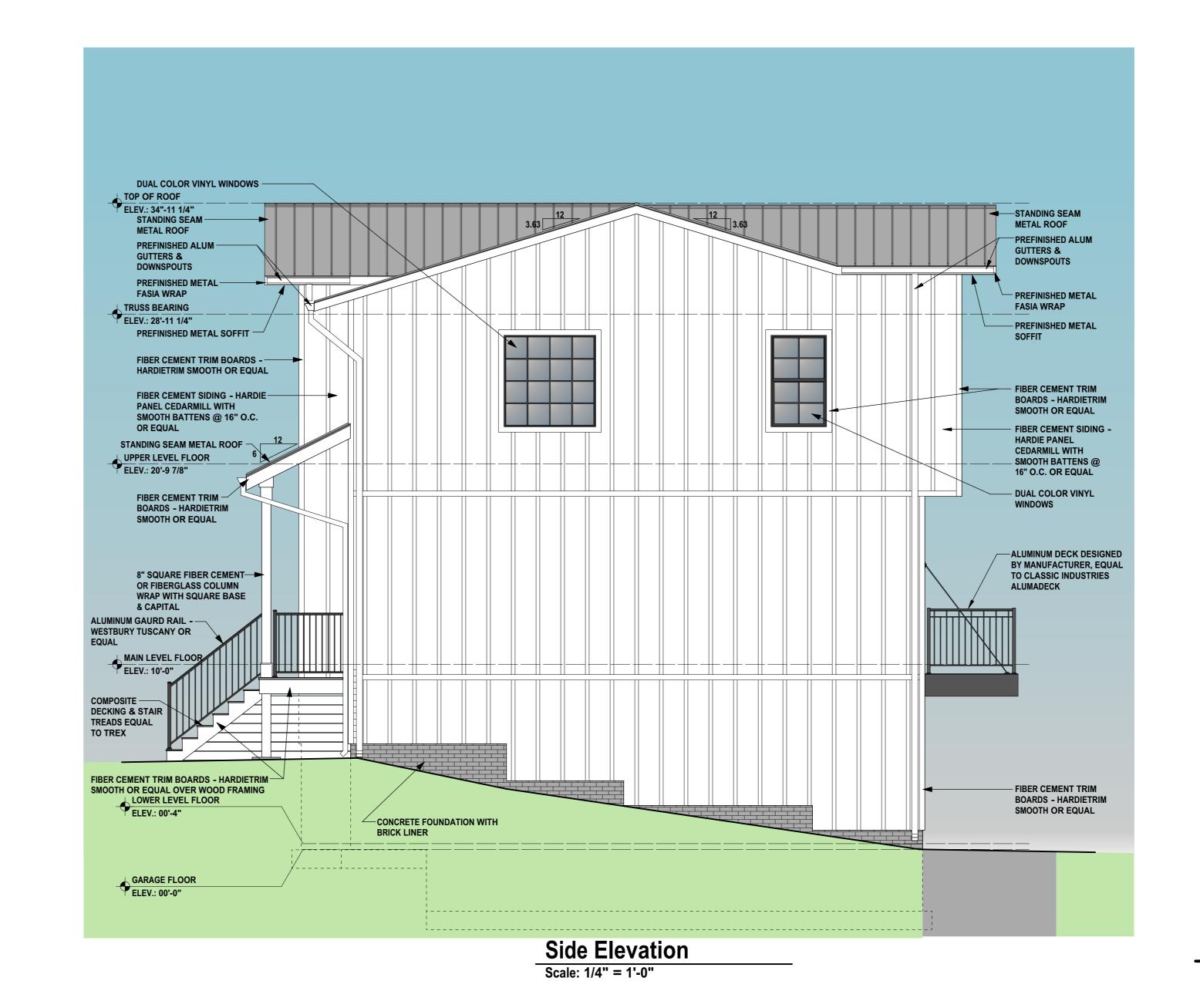


5 Unit Front Elevation
Scale: 1/4" = 1'-0"



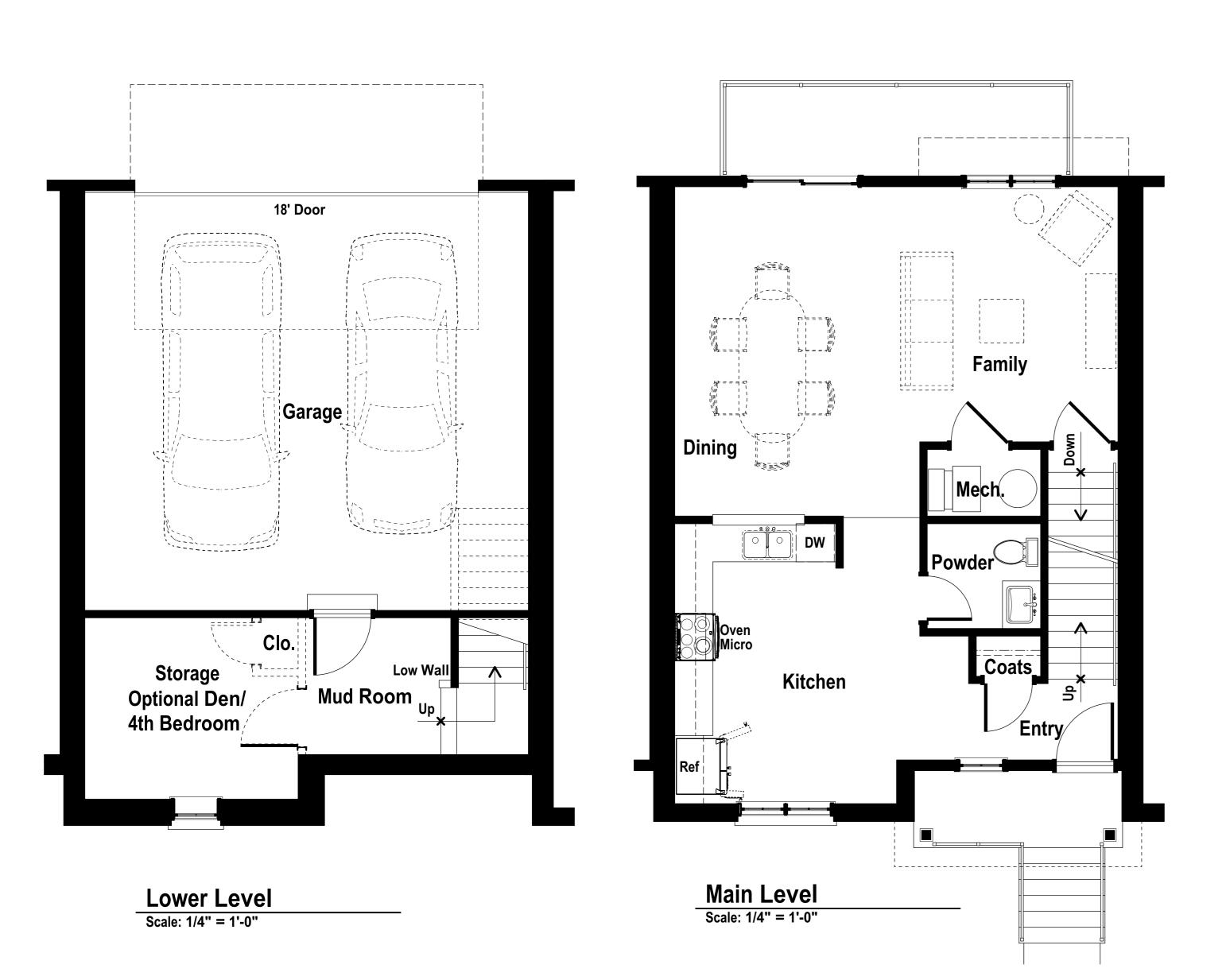
5 Unit Rear Elevation
Scale: 1/4" = 1'-0"

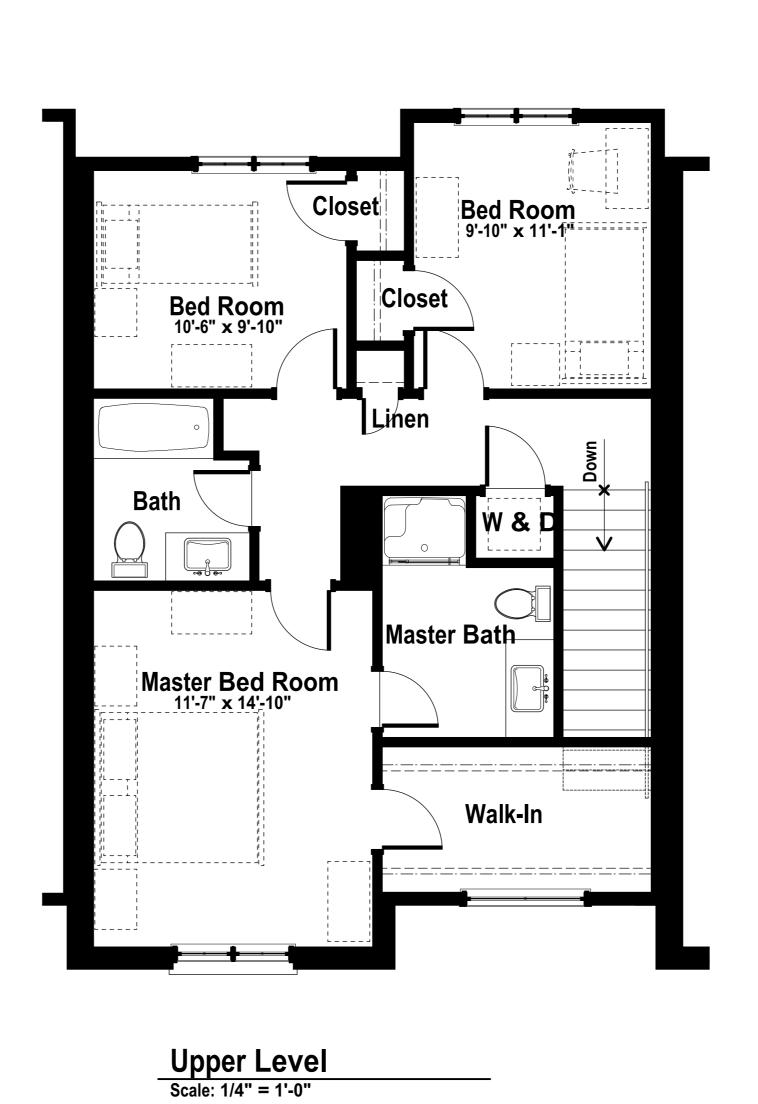


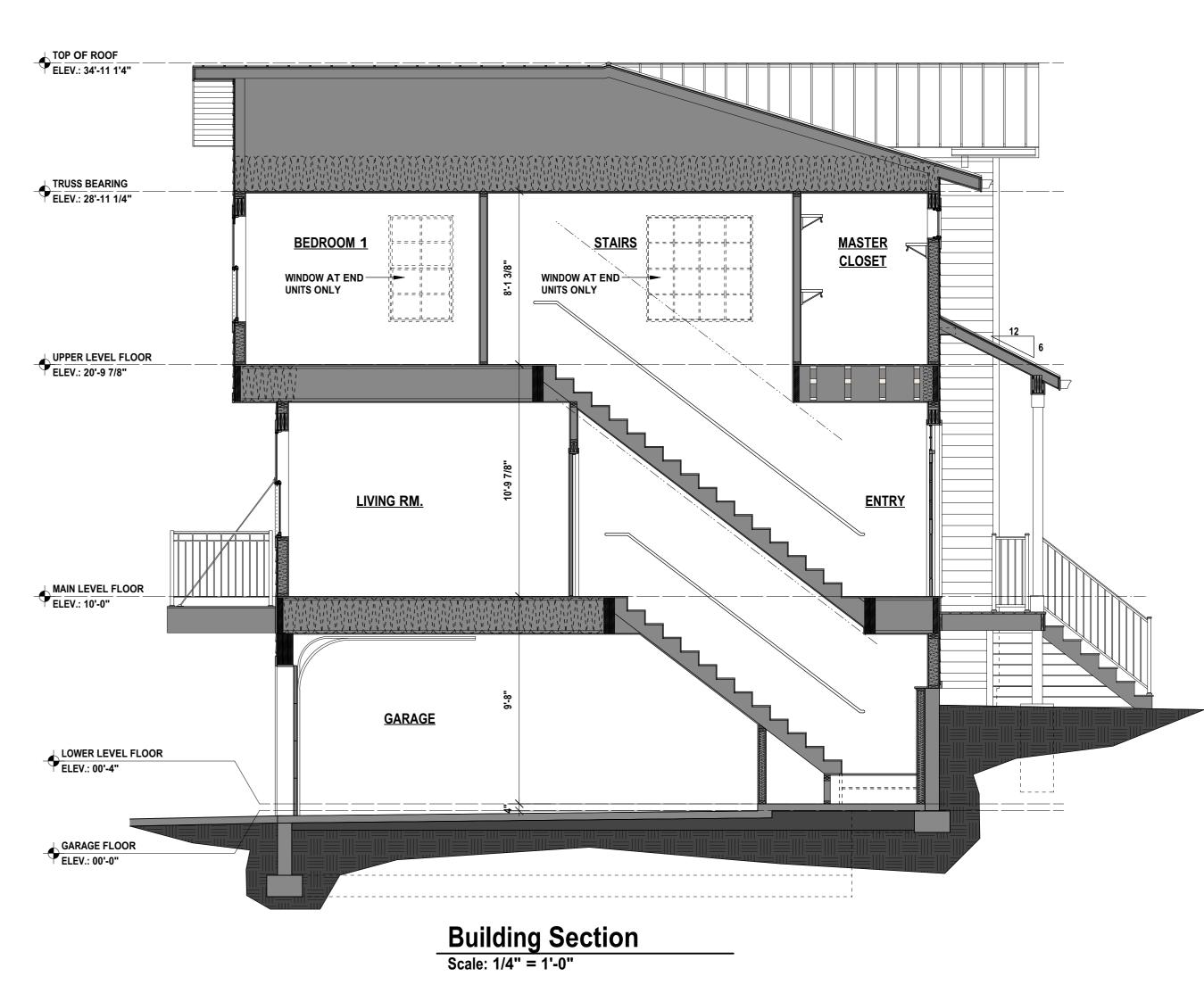


Proposed Condo Unit







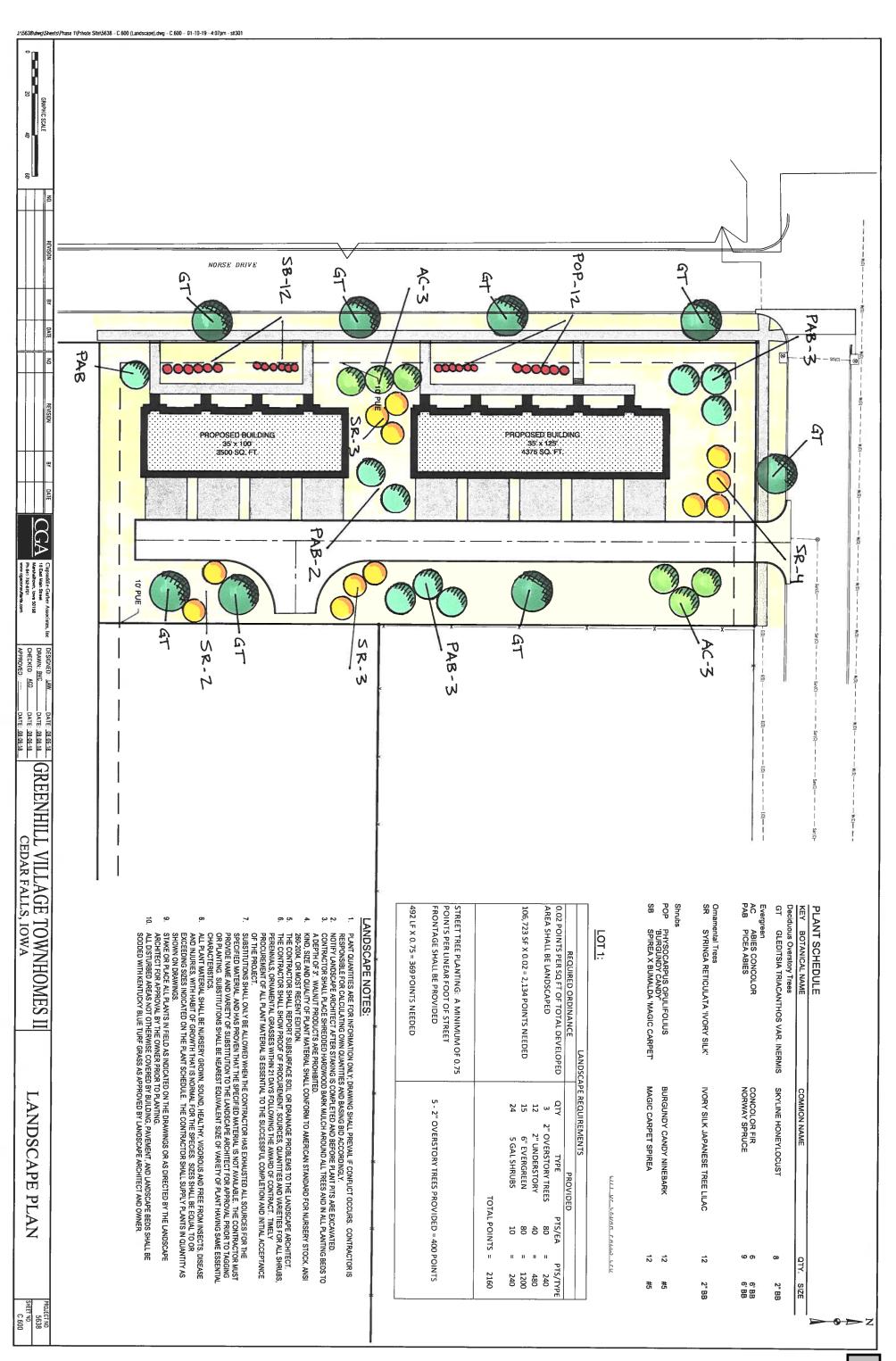




4 Unit Front Elevation
Scale: 1/4" = 1'-0"



4 Unit Rear Elevation
Scale: 1/4" = 1'-0"



Dear Planning and Zoning Committee,

My name is Michele Hanson, my family and I have lived at 1517 Athens Ct. in Greenhill Village for nine years. My children have grown up here, rode their bikes through the neighborhood, and we have enjoyed living here with our neighbors, so you can imagine our shock and dismay at the note we received January 17th inviting us to a meeting with the Planning & Zoning on January 23rd. Not only was this short notice but this letter was only sent out to only a small handful of neighbors with many that this will affect not receiving this letter at all. The big shocker in all this is that the notice was to let us know about a meeting scheduled to discuss the plan for the area directly behind our house to be developed with townhomes! This is a huge punch in the gut shocker because this land has always been in the plans to be developed into more "single family" homes! My husband is the president of our HOA and at NO time did he, the HOA, nor anyone in this neighborhood received any notice of any meetings prior to this stating that these plans had changed and that this land had been rezoned from single family homes to multifamily (Townhouse/apartments). In doing our research, and with the help of Shane Graham it was brought to our neighborhood associations attention that meetings were held on March 28 (P & Z) to ask for this property to be amended with density of townhomes and then on April 16 (City Council resolution #21,071) which approved this request! These meetings were held WITHOUT any of us in Greenhill Village ever being notified! I believe this takes away our rights to attend and share our thoughts on what happens in "OUR" neighborhood!

From the meeting minutes: (Mr. Holst verified that notifications were sent to the neighbors.)

Did it not seem strange to all involved that not one homeowner from GV was there to object to this idea or share their concerns? That's because you took away our rights which we will share with the lawyer we have retain this to represent Greenhill Village in this matter.

We ask for your help and fairness in this matter!

I sent out a questioner on our GV website to the neighbors that were supposedly sent the notice back on 3/19/18 and NOT ONE received this letter! It is highly unlikely that this many letters just happen to be lost in the mail. These notices were never sent! We are asking that the amendment made to this property from single family homes to multifamily units on April 16, when it was unanimously voted upon be the city council again without any notice to GV families be reversed!

Send out notices this time and follow the correct protocol that is put in place for a reason that allows homeowners to express their concerns when it comes to their neighborhood!

These are some of the concerns that our HOA and surrounding neighbors have with this plan:

- DENSITY of multi-family units vs single family homes in this neighborhood. We have seen the plans of 40 units / 8 complexes in the area starting with the two in the northwest corner.
- TRAFFIC getting onto Greenhill Road has been a concern for some time and is near impossible at Ashworth and Hudson turning south. Many neighbors choose to drive to Erik and Hudson which increases the neighborhood traffic and that will only be increased with Addison being extended and traffic going from Harriet and Hudson turning south.
- RUNOFF while yes you have plans for a retention pond, our neighborhood ponds have overflowed this past year alone due to heavy rains and what are the plans for this pond? It will flood and run into the neighboring yards of those who's backyards are on Addison and Ashworth and this area is a wet land area that already has standing water in it at any given time.
- BUFFER/PRIVACY the area that is in the plans to be densified by these multi-family homes (40 units/8 complexes) is an extreme density in one area without sufficient buffer between single family homes and multi-unit buildings. The fact that 3 story townhouse will be looking into our ranch home is an invasion of our privacy! The fact that the driveway to one of these units will be directly perpendicular to our yard which means that the lights from the vehicles will be shining directly into our house!
- PROPERTY VALUE we all know this will bring revenue through taxes but what about the value of our homes? We all know this answer!

Respectfully,

Michele Hanson

From: Robin Frost <frostrr@gmail.com>
Sent: Monday, January 28, 2019 8:39 PM

To: Iris Lehmann

Subject: Greenhill Village Townhomes II

Follow Up Flag: Follow up Completed

Dear Ms. Lehmann,

I am writing to express my concern at the total lack of communication the Greenhill Village neighborhood has received about the change in the bordering property development. As residents of the Greenhill Village neighborhood, we have not received ANY communication from the City of Cedar Falls about the amendment to the master plan and proposed development adjacent to our homes. The first my husband and I learned of the issue was a Facebook post by a fellow resident to the Greenhill Village Facebook Group on January 23 at 4:54 p.m., approximately one hour prior to the Planning & Zoning Committee meeting. As I did not see the message until much later, I was unable to attend the meeting that evening.

Upon reviewing the memorandum in the Community Development packet, I was disturbed to learn there had been an amendment to the Greenhill Village master plan last April (2018) to allow the area to be developed as townhomes rather than the original intention of single-family homes. I was further disturbed that neither my household, nor any of the neighbors, had received communication about the meeting or potential change prior to last April's decision. I have heard indirectly that the city's records show it sent notices about such a meeting. However, my own records (along with all of my neighbors') show that no such letter was received.

I assure you that as a new resident to Greenhill Village (as of December 2017) I was hyper-aware that development may be possible nearby, so I would have been vigilant about any communication that came from the city about this subject.

Furthermore, we did not receive a notice of the Planning & Zoning meeting on Jan. 23 where the proposed development was discussed, nor was the Greenhill Village Homeowners' Association notified. If the Homeowners' Association had received notice, the subject certainly would have been on the agenda for the annual HOA meeting which took place on Jan. 21, just two days prior to the Planning & Zoning meeting.

Not only am I dismayed at the total lack of communication from the City, but also at the proposed plan and its rapid progression through the development process. It is disturbing to read a plan that the neighborhood had no part in developing or influencing, and realize how far along the proposed development is in the process. As I read the recommendations and technical comments from the City technical staff and CFU, it appears that commencing development is imminent and inevitable. I am appalled that the City would allow this process to continue without following the proper channels, and with the knowledge that none of the neighborhood residents received the required written communication.

As you may know, adding rental units to a neighborhood can reduce nearby property values by 13.8% (American Community Survey) by reducing the desirability of the neighborhood due to safety and aesthetic concerns. Parking, water runoff, sewage and garbage are additional concerns, several of ware not addressed in the development request. Additionally, the City will want to consider the flood

that occurred in the area on Labor Day 2018 when planning for drainage and runoff. Addison Drive/Athens Court had several feet of water, which impacted the basements of many nearby homes causing property damage (inside dwellings and to outdoor landscaping), insurance claims, and complaints to the city. As this flood event had not yet occurred when the amendment was made to the plan, surely this incident should be a factor for consideration and discussion by the city and any future developer.

I am proud to live in Cedar Falls, and specifically in Greenhill Village, but disappointed by the manner in which this issue is being handled. I respectfully request a delay in this development request in order to revisit the amendment which was made without the opportunity for neighborhood residents to weigh in, research the impact and discuss with our neighbors.

Thank you for your consideration,

Robin Frost 4718 Addison Drive Cedar Falls

From: Vinod Phuke <vinodphuke@gmail.com>
Sent: Friday, February 01, 2019 11:34 AM

To: Iris Lehmann

Subject: Raising concern 8-4 Town homes recently approved by planning and zoning

department

Follow Up Flag: Follow up Flag Status: Flagged

Hello Iris,

I would like to raise my concern related to 8-4 Town homes recently approved by planning and zoning department. This was originally planned for the single family homes, this change will negatively impact on living since these town homes will be rented to tenants and we are currently facing lots of traffic and parking issues, this new plan will make things worse as a member of Greenhill village family I would like to raise my concern and strongly oppose this new plan

Best regards

Vinod Phuke

1029 Amelia Dr Unit 1

Cedar Falls IA-50613

319-610-8880

From: Larry Durchenwald <ldurch@cfu.net>

Sent: Tuesday, February 05, 2019 9:54 AM

To: Iris Lehmann

Subject: New townhouses at Greenhill Village

Members of Cedar Falls City Council;

My name is Larry Durchenwald, and my wife and I are current residents of 1525 Athens Ct. We are retired and have been part of the neighborhood since June 2013. We enjoy the neighborhood and have gotten to appreciate knowing our neighbors.

We are against the new townhouses in the area of Norse Drive, because that intersection onto Hudson Road is very busy as it is, and additional, high density, occupancy, dwellings will create a traffic issue trying to enter Hudson Road. Often, there is a line of cars waiting for the traffic light at the intersection of Greenhill Road and Hudson road, so entering Hudson Road during those situations will be very frustrating.

It is not unusual to see cars going around that tight corner of Harriet Lane and Norse Drive on the inside of the turn, no matter what direction they are going. Visibility in this corner is limited at best.

If this area is to be developed, we much prefer the addition of single family housing where properties will be better cared for by actual property owners, rather than temporary renters. We feel, also, that townhouses this close to our established neighborhood, will decrease the value of our homes.

Larry & Rita Durchenwald

1525 Athens Ct.

Cedar Falls, Iowa 50613

Sent: Monday, February 04, 2019 8:15 AM

To: 'Karmen Woelber'

Subject: RE: Greenhill Village area zoning

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Good morning Karmen,

Thank you for your email. I just want to assure you that a zoning change for apartment buildings has not been approved for this area nor is it being considered. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP

Planner II

City of Cedar Falls, Iowa Phone: 319.268.5185

From: Karmen Woelber [mailto:karmen@cfu.net]

Sent: Friday, February 01, 2019 9:15 PM

To: Iris Lehmann

Subject: Greenhill Village area zoning

Ms. Lehmann,

I live at 4808 Algonquin Dr. Unit 6. I have just been informed through my homeowner's association that there are plans to change the zoning in the area in front of the water tower. Plans to change it from single family dwelling zoning to apartment zoning. I have significant concerns about the negative impact this change would have on our neighborhood and property values. I have also been told that the city states all residents in the area have been previously informed of the anticipated change. I want to state very clearly that I have **not** received any information at all regarding this issue. The first I new of it was from an email via my homeowner's association that I received today.

Respectfully yours,

Karmen Woelber

Sent: Monday, February 04, 2019 8:19 AM

To: 'Dad' Subject: RE:

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Good morning Dean,

Thank you for your email. I just want to assure you that plans for "massive rental units" are not being considered. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP Planner II City of Cedar Falls, Iowa Phone: 319.268.5185

From: Dad [mailto:dlb51111@juno.com]
Sent: Friday, February 01, 2019 5:09 PM

To: Iris Lehmann

Subject:

Iris, I have just been informed of the proposed building of massive rental units close to where i live. The City of CF is claiming all homeowners close to that area were notified, This is false. I was never notified of such a proposition and am completely against such. The building of so many RENTAL units will no doubt decrease the value of all homes any where near that area and the traffic would be unmanageable. Also the turnover in such units is very high and would be a detriment to all homeowners around there. Please do whatever you can to stop this catastrophe from going forward.

Thank you. Dean Boyd 1315 Amelia Dr.

Drink 1 Cup Before Bed, Watch Your Body Fat Melt Like Crazy

Celebrity Local

http://thirdpartyoffers.juno.com/TGL3132/5c54d1a1abad751a168b4st04vuc

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NOT CL	ICK on links or open attachments unless you are sure the content is safe.]

Sent: Friday, February 01, 2019 3:41 PM

To: 'Diane Lantz'

Subject: RE: Greenhill Village Apartment Project

Attachments: Combined Staff Report - Greenhill Villiage Townhomes.pdf

Hi Diane,

Thank you for your email. I just want to assure you that plans for high rise apartment buildings have not been approved. The Planning and Zoning Commission will be considering the proposal for one (1) four-unit townhome building and one (1) five-unit townhome building at their February 13th Planning and Zoning Commission meeting. Townhomes are essentially single family units attached to one another to create a multiple unit building of 4-5 attached homes (much like duplexes). I attached for your reference the staff report from the last discussion of this project on January 23, 2019. It is our understanding that the developer intends to market these units as for sale.

Please feel free to contact me if you have any questions or if you still have any concerns with the proposal. Again the Commission will be discussing this project at their next meeting on February 13th and public comment is encouraged.

Kind regards,

Iris Lehmann, AICP Planner II City of Cedar Falls, Iowa Phone: 319.268.5185

----Original Message----

From: Diane Lantz [mailto:dlantz1216@yahoo.com]

Sent: Friday, February 01, 2019 3:11 PM

To: Iris Lehmann

Subject: Greenhill Village Apartment Project

I just recently learned of plans to build eight apartment buildings near the UNI water tower in Greenhill Village. No notice has even been given to the homeowners of this area.

As a residence of Greenhill Village, I'm writing to adamantly oppose this project. When purchasing my home I believed it to be a neighborhood of privately owned homes and condos. Not a neighborhood of high rise apartment buildings that will without a doubt lower the value of our properties and overpopulate this area. Traffic has already increased significantly due to the number of apartment buildings at the corner of Greenhill and Ashworth. This will only add to what already is a disappointing and sometimes dangerous situation.

The City of Cedar Falls needs to listen to the people of this neighborhood who have invested their money and made their home in a family-oriented neighborhood not suspecting for a minute that it could become a college student community.

Sincerely,

Diane Lantz Greenhill Village

Sent from my iPhone